



TOWN OF
BRECKENRIDGE

Planning Commission Meeting Agenda

Tuesday, March 21, 2023, 5:30 PM

Council Chambers

150 Ski Hill Road

Breckenridge, Colorado

5:30pm - Call to Order of the March 21, 2023 Planning Commission Meeting; 5:30pm Roll Call

Location Map 2

Approval of Minutes 3

Approval of Agenda

5:35pm - Public Comment On Historic Preservation Issues (Non-Agenda Items ONLY; 3-Minute Limit Please)

5:40pm - Consent Calendar

1. Engle Residence Addition (CC) 102 New England Drive; PL-2023-0044 13

5:45pm - Other Matters

1. Capacity Analysis 45

2. Town Council Summary

6:15pm - Adjournment

For further information, please contact the Planning Department at (970) 453-3160.

The indicated times are intended only to be used as guides. The order of the projects, as well as the length of the discussion for each project, is at the discretion of the Commission. We advise you to be present at the beginning of the meeting regardless of the estimated times.



Engle Residence
 Addition, 102 New
 England Drive



NOT TO SCALE

Breckenridge South



PLANNING COMMISSION MEETING

The meeting was called to order at 5:30 pm by Chair Frechter.

ROLL CALL

Mike Giller - absent	Mark Leas	Allen Frechter	Susan Propper
Ethan Guerra	Steve Gerard	Elaine Gort -absent	

APPROVAL OF MINUTES

With no changes, the February 21, 2023 Planning Commission Minutes were approved.

APPROVAL OF AGENDA

With no changes, the March 7, 2023 Planning Commission Agenda was approved.

PUBLIC COMMENT ON HISTORIC PRESERVATION ISSUES:

- No public comments and Mr. Frechter closed public comment section.

FINAL HEARINGS:

1. Thomas Residence Addition (SS), 314 Lincoln Avenue, PL-2022-0524

Ms. Szrek presented a proposal to locally landmark and rehabilitate an existing historic residence, construct a basement and basement connector, interior remodel, and add a garage to the north of the primary structure. The following specific questions were asked of the Commission:

1. Does the Commission agree that a window opening predated the doorway opening on the southern façade and that the proposed design complies with Design Standards 23 and 76?
2. Does the Commission support the recommended point analysis?
3. Does the Commission have any additional questions or comments on the proposed project design?

Commissioner Questions / Comments:

Mr. Guerra: No questions.

Mr. Gerard: No questions.

Mr. Leas: No questions.

Ms. Propper: No questions.

Mr. Frechter: I have two questions for the storage space; if the applicant makes that bedroom and or bathroom would that require another review? Or affect parking requirements? (Ms. Szrek: Yes, it would affect some of our Codes. You are correct about the parking, they would have to provide an additional parking spot if it were to be changed into a bedroom. Right now, they are maxed out on their parking. Further, they would be required to have egress.) The setbacks from the north side of the garage to the adjacent homes window wells; it is a foot or 13" inches. Do the window wells not count as part of the required setback? (Ms. Szrek: Window wells are at or below grade so they would not be a part of the setback. Building Department and the Chief Building Inspector preliminarily okayed it.)

Mr. Guerra: I would ask about the door on the east side. You found the historic photograph dating back to the 30s. Were you able to identify when that was changed into a doorway?

Sonny Neely, Neely Architecture, Applicant:

It must have been a repurpose or had a stair associated with it. It is 24" inches above grade. I think that they might have done it to rent out the back bedroom.

- Mr. Guerra: I saw you did some forensic excavations. Did you look at that? I am just curious why we are seeing a door opening.
- Mr. Neely: It is probably similar to what happened on the front of the residence. I believe they renovated the upstairs separately from the downstairs. They put the wall in between the stair and living area. Then they put a fireplace in that corner at the front of the house. That necessitated the closing off of the other door, the original 36" door. Then I think when they decided on opening it up as one living unit, they put that arched opening in the wall to partition the two spaces. They may have done it the same way in the back at some time where they just sublet the house. (Mr. Guerra: You are going to make that one door go away?) Yes, it will go away just like the doorway on the front. It is obvious that the window went to a door, the brick is cut and chipped and irregular. I think it is very obvious that there were two 36" doors; one in the front and one in the back.

The hearing was opened for public comment; there were no comments and the hearing was closed.

Commissioner Comments:

- Mr. Guerra: I have no further comments. 1) I do agree the window opening predated the door opening and I think it complies. 2) I agree with the recommended point analyses. 3) No other comments.
- Mr. Gerard: 1) Yes, I agree. I thank Sonny and the department for their investigative work. 2) Yes, I support the recommended point analyses. I think this is an important residence. It is part of the Historic District and a contributing structure. It is certainly eligible for landmarking. I support the project; I think it's a great amount of work. Adding the extra excavation to provide storage space underground is a no brainer; it may make your other work easier.
- Mr. Leas: 1) Yes 2) Yes 3) this is a fascinating renovation because of the scope of it.
- Ms. Propper: 1) Yes 2) Yes, I agree with the point analyses. 3) No additional comments.
- Mr. Frechter: 1) Yes, I do agree the windows predate the doorway. Glad to see it restored to how it was originally built. 2) Yes, I do agree with the point analyses. 3) No additional comments.
- Ms. Szrek: I want to add a point of information that staff would like to make a motion to add the additional finding to the project in regard to the mention of heated paving on the plan set and the ability of the applicant to come back to claim the additional banked positive point (+1) to add up to 500 square feet of heated paving if code allows at the time of application. We just want to clarify that the heated paving is not approved under this permit.

Mr. Gerard made a motion to approve the Thomas Residence Addition with an additional finding that the snowmelt on the plans are not approved with this approval and the applicant can return to work with Staff to utilize the banked positive one point later if allowed by code at the time of application, seconded by Mr. Guerra. The motion passed 5 to 0.

PRELIMINARY HEARINGS:

1. Breckenridge Grand Vacations Gondola Lots Site Specific Development Permit (CK), 350, 355, 415 S. Park Avenue, PL-2022-0208

DISCLOSURE: Ms. Propper disclosed that in June of 2021, she and her husband wrote a letter to the Town Council in support of the Master Plan for this area. This was prior to her joining the Planning Commission and prior to any application for site-specific development. The Commission agreed there was no conflict of interest and Ms. Propper remained for the hearing.

Mr. Kulick presented a proposal for a site-specific development permit application for 57 condominiums, 24,149 sq. ft. of commercial space, 80 hotel units, 10 duplex units, 9 employee housing apartment units, and 3 townhome units over 10 buildings. The proposal also includes roadway and pedestrian improvements, including a roundabout at the intersection of Park Avenue and French Street, a new gondola, a new park and pedestrian plaza, and a 1,076-space parking structure. The following specific questions were asked of the Commission:

1. Building Height:
 - a. Is the Commission supportive of a building height exemption for the focal towers on Buildings 1 and 2, and Building 1's emergency stair tower?
 - b. Does the Commission find there is enough variation in height throughout the site?
2. Architectural Compatibility:
 - a. The parking structure's eastern façade is designed with 78% non-natural materials. Based on past precedent, staff recommends the assignment of negative six (-6) points. Does the Commission agree?
 - b. Does the Commission agree that flat roofs are allowed without the assignment of negative points for Mixed Use Buildings 1, 2 and 3 under the master plan?
 - c. Does the Commission believe the style of windows on the eastern façade of the North Gondola Townhomes should be adjusted to better complement the adjacent Historic District?
3. Site and Environmental Design: Does the Commission agree the proposal is sufficiently buffered due to the increased tree retention along South Park Avenue and the proposed landscaping plan?
4. Landscape Plan: Does the Commission believe that an award of four (+4) positive points is deserved based on the revised landscaping plan that includes the retention of additional mature trees and increases the quantity of trees on the east side of the North Gondola Lot, adjacent to the river?
5. Additional Comments: Does the Commission have any additional questions or comments on the proposed plan?

Commissioner Questions / Comments:

Ms. Propper: I was wondering if the new gondola would be built at that time so there are no problems with pedestrians crossing Park Avenue? (Mr. Kulick: Yes, between the parking structure, roundabout, the gondola, and the circulation in that area these are all required to be completed in the first phase of development.) What about the EV-capable spaces in the parking structure, would there be any positive points with that? (Mr. Kulick: I did overlook the chargers with the amount of things to review for this meeting. They may be eligible for positive points under Policy 33.)

Mr. Leas: Question about the parking garage. It is going to be a similar situation where Vail Resorts owns the land and is leasing it to who? To the Town? To BGV? (Mr. Kulick: It is my understanding that BGV has a lease agreement with Vail Resorts.)

Mr. Gerard: There is going to be three places, because we have three lots, that parking charging stalls could be installed. Will there be separate analyses on receiving positive points at each individual location? Can they max out the positive points three times across the development? (Mr. Kulick: Similar to architectural compatibility; on this they can only get the maximum number of points across the entire development). Staging goes the way it looks like it has to go, the immediate loss is going to be to the Gold Rush Lot parking. Is that part of the required parking that is to be provided? (Mr. Kulick: That is. Based on precedent, there is some allowance to disregard that requirement to provide those spaces while the area is under construction. There are several hundred spots that will be interrupted. But we have precedent from our own project.) My final question. Unbroken rooflines, on any of these buildings? There are some pretty long runs. (Mr. Kulick: That

is something that staff will need to review. There can only be one negative (-1) point assigned.)

Mr. Guerra: My questions regard the flat roofs, and the language that is used. If the question is allowing flat roofs, I think we should review the language. You did not address the windows on the eastern façade of the North Gondola townhomes. On the flat roofs, you are asking us if we agree that those types are allowed. I would want to go back to that language. (Mr. Kulick: The way it is written is very awkward “These buildings may have both gabled and hipped roofs, there may be flat roof types that may also be used for outdoor decks.” It does not say that flat roof types may be used as decks. It makes it seem like a third that is being permitted.) I think that flat roofs can be a component, but not a main component in the design. So I ask again how are we interpreting that and I think that is what you are asking. (Mr. Kulick: I think that is why we specifically asked the Commission. I would agree that building three sounds like it is more of a component. For building one and two, it is Staff’s opinion that it is allowed.)

Mr. Gerard: We had the discussion about rooftop decks. It was a hot topic from an applicant downtown. It was specifically part of our thought process that in knowing these would be used for rooftop decks so they built some language in. I am not sure they built in language that all the roofs can be flat.

Ms. Propper: Building two is along the river parallel with the townhomes, would we also want the style of windows to reflect the adjacent Historic District? (Kulick: The design language for buildings one and two were not specific about the transition into the Historic District area. It says the townhomes will utilize roof forms and some traditional elements of the homes in Breckenridge. It is vague; it does not go back into the specific building elements.)

Mr. Gerard: Roofs, there was a specific conversation that I recall about the idea there could be many roof tops utilized as entertainment type decks, where people would be on those rooftops, and we were still struggling with roof top decks in the downtown zone. There was a decision made to specifically permit those to pull them out of any later objections to having rooftop decks. (Mr. Guerra: My question is if that is the language from the original master plan?) (Mr. Kulick: Yes, this is from the approved master plan. I think that it was part of it. There was discussion on roof top decks, but it had just as much to do with roof top decks in the actual historic district on Main Street. Activating the roofs with hot tubs, pools was an important element of the design so it was written into the Master Plan.) And it was clearly disclosed that the applicant wanted roof top decks for entertainment type purposes.

Mr. Leas: The big question that we have not been cleared up, it allows gable roofs and allows flat roofs. It appears that we have 100% flat roofs. (Mr. Kulick: It is a question being a relative policy, they get negative points. I think guidance is important depending on what they do to modify them. In terms of points, they are already receiving the maximum points for architectural compatibility. The flat roofs and windows of the townhomes are almost a bonus point for them.)

Mr. Frechter: I agree with Susan, the eastern façade needs to complement the Historic District. In hindsight, we didn’t think of that during the planning of the master plan. (Mr. Kulick: I think we acknowledged that the townhomes would be a smaller building, so that was the bridge to the large buildings. I think we acknowledge that building one and two were going to be the largest. We want there to be a diversity in height.) There are going to be safety barriers along HWY 9? Is there adequate space to clear snow from the development and the highway? (Mr. Kulick: Those items have been discussed by Engineering. Those developments would be decided with Engineering and CDOT.)

Mr. Frechter: When we did have the previous meeting with the applicant, we had a lengthy discussion about the design of the project. We gave them a lot of feedback towards the transitional

designs influenced by other towns and areas. We gave positive feedback to those designs and those were all flat roofs.

Mr. Leas: Based on what Allen brought up about the right-of-way on the west side of Park Avenue, that sidewalk is going to go from one roundabout to the next roundabout and connect to the ski back? (Mr. Kulick: They are obligated to go to the other side of the roundabout to make the connection. They are going around, but they don't have a street crossing.) I think it is a good idea, but I wanted to understand.

Graham Frank, Breckenridge Grand Vacations: Mark, to answer your question on ownership. A single entity under BGV owns both the South Gold Rush and North Gondola Lot. We have a long-term land lease on the North Gold Rush Lot and then we would condominiumize the boxes where the employee housing structure is located. It would be condos inside the air rights; very similar to what we did on the lot up at Peak 8. (Mr. Leas: and BGV would manage and collect for parking?) Correct, we would manage and collect the revenue.

Bill Campie, DTJ Design: This project been going on for years now, I think that it has gotten better over time with input from the community. We did intend to allow flat roofs in the development and that is why it says they are allowed. We just separated it out and put an additional statement regarding the roof top use. That is why it is separated the way it was. Those other rooftops did not have the additional consideration. It was never an intention to and that is why it doesn't say one roof type would be dominant. It allows these types and rooftop decks. DTJ and BGV have gone through multiple rounds of design revisions, predominantly driven by community, commission and staff input. This is the third version. Each time have taken the discussion of the planning commission as an assignment. We have prioritized tree preservation in areas discussed by the Commission. We modified the circulation per the discussion and implemented landscaping along the river corridor. We made a massive reduction in heated surfaces and outdoor water. Architectural style changed a lot through the project; during master planning we had a lot of ideas and concepts. We quickly pivoted based on feedback. I think that we have come to a place that both parties really like. We have tried to find a way of infilling this in Breckenridge. Building heights were up and we now have tried to meet those. The master plan does mention to vary heights. We have varied the type per building and in terms of Code and introduced stepping down the buildings. The park: we just need to move a few feet and we can create the barrier. The encroachment of the easement, that discussion has begun because they want that too. The internal circulation has multiple solutions for the bus route turn around and will continue to work with Engineering. Regarding the negative points, our intention is to have no negative points. Our intention is to meet that section of the Code; whatever that is. Wood siding on the parking structure, we will do. We feel confident about removing the points. Again, this is a resort project, it will be difficult to avoid the points. We will have to offset. We do have the maximum number of EVSE points, located in two locations. BGV has a value system already. They have a REMP program they are implementing a lot of things that the Town is looking at doing. Our expectation is to finish this with positive points, not just zero.

Ms. Propper: No questions.

Mr. Leas: We just had an energy work session. The discussion around availability of real estate for the implementation of the solar. You should talk to staff about overcoming some of the energy points through that avenue. I did have a question about the site plan. Specifically, how the hotel works with the building across from it. It looks like there is a grander entry for the other building. How do these buildings interact with each other?

Mr. Campie: There is a discussion about shared amenities, check-in, or operations. Both will have lobbies, but one will be more centrally used as a universal check-in. It will be similar to the parking; it is consolidated as well. You would have a shared operational system across the whole district.

- Mr. Leas: If you are going to have multiple locations of activity, and guests have to carry luggage from one building to another, I think it is important for the Town to realize that the heated paving is a safety issue. (Mr. Campie: The Commission can give us the allowance for heated surfaces.)
- Mr. Gerard: How do you think that you would respond to the comment on more historic type windows and features on the townhomes stepping down from the hotel property? (Mr. Campie: We are not going to fight some battle about the size of windows. The larger windows are better in terms of living perspective and energy efficiency.) Are you still willing to be engaged in conversation with the Town of Breckenridge on these items? (Mr. Campie: Yes.)
- Mr. Guerra: Flat roofs will be highly visible coming down in the gondola, are you planning on using ballast rocks or something similar? What will we be looking at on top of the roofs? (Mr. Campie: We have not gotten to that level of detail yet. But that is a marketing window along the gondolas, so we want to make sure that it looks good.)
- Mr. Frechter: I do not have any questions. I think that is great stepping down with the garage and changing the structural steel. (Mr. Campie: What is shown there is not a structural element; that was a design concept. It is open for sure, but that will be a concrete structure.)

The hearing was opened for public comment.

Frank Robinson, Woods Drive, Board member of HOA: Going south of Park Avenue, will there be a right-hand turn lane into the parking garage? I think that we should have one, there is going to be a lot of traffic going into the parking garage. I think we should have two lanes going out of the garage going to the roundabout, to facilitate the cars dispersing from that area. Some going right, some going left. I want to thank BGV for connecting the sidewalk to the skiway, that is going to eliminate a lot of traffic along Woods Drive. Skiers ski down or walk down to get to the parking lot. And the parking garage adding spaces would be a big help. The south Gold Rush lot is Zone 3 for short-term rental purposes. That property is Zone three, very limited on short-term rental. I think it should remain Zone Three just as Woods Drive. So, I hope that we don't have a switch of hands and since Vail or BGV has it we will make it Zone One. Our concern is that we want it to remain Zone Three for rental purposes.

Margaret Douglass, 105 Woods Drive: The safety issues with the parking structure and access only on Woods Drive. What if there is a fire or EMS is needed we are going to be trapped behind the traffic of the enormous parking structure. Thank you to Town Council, I was able to view the traffic study and the study area does not include Woods Drive above the parking structure. We are 25 units up there that I feel are being overlooked in how this may affect our safety and access. I am happy to hear about the sidewalk coming from the skiway down. Because I think if I were with my family and figured out instead of skiing all the way down to the tunnel, I could just ski down Woods Drive, I would. I do understand that Woods Drive is a public street and does not belong to our neighborhood, but it is also not a ski way.

The public comment period was closed.

Commissioner Comments:

- Ms. Propper: 1A) I am supportive of the Building Height exemption. 1B) Yes, I think there is enough variation in height. 2A) I agree that there needs to be more natural material on the facade. 2B) I have looked at the language over and over. I think it does permit flat roofs. I am saying yes to that. 2C) I would like to see the windows on the townhome adjusted. 3) I do think the addition of trees provides sufficient buffering. 4) I do agree with the four positive points. I have no additional comments.

- Mr. Leas: 1A) I do concur. 1B) I think there is variation in the height. I would like to see more. 2A) Yes, the parking structure. 2B) The question still has not been answered as the plan is present if that is the appropriate language. I would say no to that. Maybe we have some variation there. 2C) the site environmental design, I concur. The landscape plan, I like that. No additional comments.
- Mr. Gerard; 1A) I am supportive of the building height exemption. I think that helps to break up the facades of the building. 1B) I think there is enough variation in height. I would change the height of the hotel which is one big, long building with an unbroken ridgeline. 2A) I think there should be a reduction in non-natural materials. 2B) The flat roof issue is interesting, and I am not sure if all should be flat roofs. 2C) I think that there should be some adjustment to the Townhomes to complement the adjacent historic district. The windows should be redesigned a little bit. 3) The increased buffering is appropriate, and I approve of that. 4) The landscape plan should receive four points for the design around the mature trees that were allowed to stand. My comments are I think that we are getting better each time. Thank you and great progress.
- Mr. Guerra: 1A) I am supportive of the height exemption for the towers. 1B) I do find that there is enough variation in the focal towers and step down of other buildings. 2A). I do agree that we need to see more natural materials on the parking structure façade. Six points are warranted. 2B) It is a question to me because it was brought up. It is not clear that everyone agrees. Staff does not and have asked us. Are they allowed? The applicant says that they are. The language, to me, is vague. I won't over comment on that. I like the design so I would agree that flat roofs are allowed without points, I would like to see some ballast rocks or something. 2C) I agree that the windows need to be readdressed. 3) I like the buffers, moving the parking garage helped a lot. 4) On the landscape plan, I was excited to see the inclusion of those extra trees. I am supportive of the four points.
- Mr. Frechter. 1A) Building heights. I agree with the exemption for the towers, particularly because the stair tower is setback into the middle of the building. It is setback; I don't think it will be noticeable. 1B) I think that there is a lot of variation in building heights. I would welcome more, but what is there is acceptable. 2A) I agree on the negative six based on precedent. If the applicant can reduce the non-natural that would be great. 2B) Based on the master plan, I think flat roofs are allowed. 2C) Based on the master plan on what we agreed to, the townhomes would complement to this historic district more. A modification would be necessary. 3) The buffering, I like. 4) I think the landscape plan, based on precedent, should receive four points.

WORK SESSIONS:

1. Stables Village Master Plan

Ms. Crump presented a proposal for a new for-sale workforce housing development located in Planning Area-A (PA-A), planned for the Stillson Patch Placer tract, located on the south side of Wellington Road on the former site of the Breckenridge Stables. The Master Plan proposes 61 workforce housing units, including 5 single-family units, 38 duplex units, and 18 triplex units. The Planning Area-B (PA-B) of the tract will remain allocated to governmental uses, such as open space, recreation, public works storage, snow storage, and solar energy production. The following specific questions were asked of the Commission:

1. Does the Commission have any concerns with the methodology used to establish the existing grade across the site?
2. Does the Commission agree with the analysis that considers the triplex units like duplex units regarding parking requirements?
3. Does the Commission support the proposed fencing in the Master Plan?
4. Does the Commission have any other comments on the draft Master Plan?

5. What comments do you have on the preliminary proposed architectural designs for the single-family, duplex, and triplex structures?

Commissioner Questions / Comments:

- Mr. Gerard: Do we have a legal definition of either duplex or triplex? I did not do a word search. (Mr. Kulick: We do have a duplex and multi-family definition which is three or more units in the same building. I think it worked when we only had parking minimums. You could have five townhomes in a row: each having parking and land. It creates an equity issue; one unit will have one and others will have two spaces. It's easy to miss both ways by adjusting it either being above the maximum or below the required amount. We should have added a townhome, single family-attached or a duplex definition during the parking maximum code change. Since this is a Town project, this maximum requirement can be waived.)
- Mr. Leas: No questions.
- Mr. Gerard: What is the bus circulation there? (Ms. Crump: There is an existing public bus stop that is close on Wellington Road and Bridge Street, on the north side of Wellington. No public transit will come into this development.)
- Ms. Propper: No questions.
- Mr. Frechter: In the Wellington/Lincoln Park, there are rules about not parking on the pads in front of the garages to allow access for the alleys. These are engineered so residents can park there and allow for emergency vehicles and such. (Ms. Crumps: Yes, the ROW change has allowed for space for parking and additional feet for vehicle clearance to stay out of the sidewalk.) What is the rationale for the perimeter bike trail? (Ms. Crump: For adequate drainage, they needed a bioswale. I think it was also a benefit to link these areas to the existing trails to the north and south.)

Lindsey Newman, Norris Design: The vision for Stables Village is to create a carbon neutral and net zero workforce housing development focused on green infrastructure and sustainability. We want to include community gardens, some nature play, native plantings and grass. We have a lot of existing disturbances on this site and limited access from Stables Drive. We will make a minimal change to the bike park parking lot connection. The site does have approximately forty feet of elevation change. It is important to see how the topography changes on the site. We are really focusing our development in the main core and decreasing as it goes to edge.

Elena Scott, Norris Design: I wanted to talk about how this development relates to Lincoln Park and the Wellington. We wanted to create a new framework for this neighborhood fronting on some form of open space. The internal courtyard spaces, bioswales, and forest service lands. The connection between Stables Road, this neighborhood, and Lincoln Park can create a buffer between proposed homes and the existing residences. We want to ensure the proposed greens to be as large or equal to the existing. Each has a different theme which is really cool, but also connected. Pedestrian connection into Lincoln Park and the preservation of the forested areas in this zone have been prioritized. When looking at the units as a whole, middle income with bedrooms for growing families, each unit having an EV ready space, and a covered parking space. Our bedroom count is 175. The bioswale itself does offer a few environmental advantages like runoff volume, water quality, and groundwater facilitation. We are looking at all components of the site to improve it as a whole. We do have 22 surface spaces provided across the neighborhoods; parking for our guests. The recycling and compost center is for the entire neighborhood as a whole. This replaces the existing recycling center on the Stillson site and also introduces composting. We have established an average grade and have a level playing field for a baseline to which we will measure height. We have both uphill and downhill units. Important talks with Engineering include traffic control on Stables Drive as well as connectivity. We have proposed to flip the parking at the Bike Park so it is adjacent to the trail; that has also been done with Engineering. The red represents the five foot walk, the green and orange

were developed with walking dogs and getting bikes up to that trail; this will be done with signage. Huge benefit to the community and neighborhood. The fencing is a more modern approach. The fencing on plan B and plan A is to provide buffering. The three foot fence is permitted in the rear of the property for dog runs and similar situations.

Suzanne Allen-Sabo, Allen-Guerra Architecture, Stables Village LLC: We are at a schematic model stage with the architecture. We need to look at the placement, grade, and the solar calculations. This would be the first subdivision in Colorado that is both, net zero and carbon neutral. Solar is definitely driving the architecture and how these roofs are oriented on the site. We will be using naturally colored materials on the buildings and roofs. We will have both natural and non-natural materials on the buildings. We are potentially getting negative points on the Master Plan level. Andy will speak about the site and how they sit on the site.

Andy Stabile, Allen-Guerra Architect: There are some buffers around the development. I look forward to Norris making this looking as natural as can be. The architecture itself is aspirational at this point. We wanted to give an idea of what we are going for. This slide shows the home is designed for net zero. Our Preliminary calculations result in 25 to 40 PV panels on the roof. To get those square footages we have large mono pitched roof design. Most will be south or southwest facing roof structures to maximize the single pane and some of the overhangs we get dual use; for roof space and covered decks. You can see that most of the units the roof will face due south. Initial solar analyses show we should get really good solar production from this site. The roof pitches are low because the site steps up as you move north to south; residents will look over the units to the north. We lose views because of the hillside behind. Most views will be North and West. In some areas we were able to bump up the roofs to get some more light in units. The garage access is opposite to the view side; the design can be flipped depending on the view and hillside. All have storage lockers outside the front door, storage in the garage. The main living will always be on the second floor so you get the theatre view over your neighbor to the north. Right now, we have five-unit types; down slope unit, flat lot unit, up slope unit, single family homes, and the tri-plex units in the middle. We are working on a few color palettes, so nothing gets repetitive. We do have some non-natural materials, metals, and other wainscoting. We will continue to refine these elevations and drawing. Things will be changing as we refine with feedback, but that is what we are shooting for.

Mr. Truckey: Just to clarify, when these individual units come back for development permits they are not going to be coming to the Planning Commission. The individual units will be administratively reviewed by staff. You won't be seeing the final detailed architectural plans.

Mr. Leas: I am very confused by this site plan. In order to establish the site, is there going to be consistent grade across the entire everything, or lot specific? (Ms. Crump: They took the topo lines and made them straight to give an average. Our Code right now allows a 35' height limitation. There is an exception in the Code that allows the use of an established existing grade on sites with heavy site disturbance from mining by using an average slope. So we are establishing an average slope to use as the existing grade. This will not be the proposed final grade or the over-lot grading plan. This is just showing the average grade existing on site.) So, this is used as a tool to establish what height is? (Ms. Crump: Exactly.)

Mr. Gerard: To Andy or Suzanne, some of these roofs create a valley. Is this going to create a problem for snow or ice. (Andy: We have designed this to our advantage. We can manage the water and put it where we want it to go. The runoff will go off the side instead of the front to mitigate the issues.)

Commissioner Comments:

- Mr. Gerard: 1) Yes, I agree with the methodology of establishing the grade. 2) I agree that it makes more sense to call the triplexes “duplex units” for purposes of parking. My preference would be to make a quick fix to the definition in the Town Code. Since this is a town project, they can do what they want. 3) I think the fencing is okay. I understand the need to divide the uses. I like the contemporary horizontal fencing. 4) It is well thought out. The view looks atop of another view. Everyone gets something unique. The architectural designs are very nice, pretty, and will be great homes.
- Mr. Leas: 1) Now that I understand the grade, I think that makes sense. 2) I do support the triplex/duplex parking decision. 3) I do understand the fences. 4) I don’t have any specific comments. I am not fan of mountain modern. I think these are going to be appropriate for the site. I just hope the architecture in Breckenridge does not turn into something that we are doing only to comply with sustainability. Architecture should stand on its own. I think that these don’t look like Breckenridge to me.
- Ms. Propper: 1) I am fine with the methodology to establish the existing grade. 2) I like the idea to consider tri-plex like duplex for parking or it would be a problem. 3) I am fine with proposed fencing. 4) I understand Mark’s comments on Mountain Modern, but I think it works for this project.
- Mr. Frechter: 1) I don’t have concerns of methodology for grading. 2) I think the duplex/triplex is okay, parking is important. Everyone should be able to park in two spots. 3) I support the fencing decision. I would recommend going to the upper part of Wellington to see buffering and even down to Vista Point. This is a great plan. I applaud the goals of carbon neutral and net zero.

OTHER MATTERS:

1. Town Council Summary

ADJOURNMENT:

The meeting was adjourned at 9:11 pm.

Allen Frechter, Chair



TOWN OF BRECKENRIDGE
COMMUNITY DEVELOPMENT

Class C Major Single Family Development Review Staff Report

Project Title:	Engle Residence Addition	
Proposal:	Proposing a 1,150 sq. ft. addition consisting of an enlarged kitchen, breakfast nook, two bedrooms, and two baths. One existing bedroom is to be combined with another and the laundry room will be enlarged. The addition will match the existing exterior appearance and is located at the rear of the home where it is less visible from the road. All windows will be replaced and a new front entry deck will welcome visitors to the new front door.	
PC#:	PL-2023-0044	
Project Manager:	Clif Cross, Planner I	
PC Meeting Date:	March 21, 2023	
Preapplication Meeting Date (REQUIRED):	February 2, 2023	Clif Cross
Date of Report:	March 17, 2023	
Property Owner:	Paul and Sydney Engle, Engle Family Living Trust	
Agent:	Darci Hughes / Riverbend Architecture & Planning	
Proposed Use:	Single Family Residence	
Address:	102 New England Drive	
Legal Description:	Lot 8, Block 1, Warrior's Mark West Filing #5	
Area of Site in Square Feet:	16,328 sq. ft.	0.37 acres
Existing Site Conditions:	The rectangular lot narrows towards the south and does not feature a Building or Disturbance Envelope. The lot is bordered by two adjacent parcels. The lot is covered with mature tree growth and shrubs. There are no easements within the parcel or surrounding the parcel. The existing single-family residence home, built in 1989, is 3,159 sq. ft. and contains four (4) bedrooms and four (4) bathrooms. It is two stories, and features primarily wood siding, stone facade exterior, and asphalt shingle roofing. The existing structure sits at 25' from the front property line to the north, the west side setback is 23' to the existing garage wall, the east side setback is 12' to the existing wall, and the rear setback to the south property line is 58'.	
Area of building:	Proposed New Square Footage	Existing Square Footage
Main Level:	495 sq. ft.	1,856 sq. ft.
Upper Level:	660 sq. ft.	841 sq. ft.
Total Density:	1,155 sq. ft.	2,697 sq. ft.
Garage:		462 sq. ft.
Mechanical:		
Total:	1,155 sq. ft.	3,159 sq. ft.
Total sq. ft. after project completion:	4,314 sq. ft.	
Code Policies (Policy #)		
Land Use District (2A/2R):	30-5	
Density (3A/3R):	1:2 (8,164 sq. ft.) or 4,500 sq. ft. whichever is less.	Proposed: 4,314 sq. ft.
F.A.R.:	1:3.78 FAR	
No. of Main Residence Bedrooms:	5 bedrooms	
No. of Main Residence Bathrooms:	5.5 bathrooms	
Height (6A/6R):	27.0 feet overall	
*Max height of 35' for single family outside Conservation District <u>unless</u> otherwise stated on the recorded plat. Tallest height of the existing structure is 25' 5".		
Lot Coverage/Open Space (21R):		
Drip line of Building/Non-Permeable Sq. Ft.:	4,320 sq. ft.	26.46%

Hard Surface/Non-Permeable Sq. Ft.:	1,232 sq. ft.	7.55%
Open Space / Permeable:	10,776 sq. ft.	66.00%
Snowstack (13A/13R):		
Required Square Footage:	308 sq. ft.	25% of paved surfaces is required
Proposed Square Footage:	548 sq. ft.	(44.48% of paved surfaces)
Outdoor Heated Space (33A/33R):		
	NO	
Parking (18A/18/R):		
Required:	4 spaces	
Proposed:	4 spaces	
Fireplaces (30A/30R):		
Number of Gas Fired:	2 Gas Fired	
Building/Disturbance Envelope?	None	
Setbacks (9A/9R):		
Required setbacks for the Warrior's Mark West Filing #5 are the same as what was specified under Summit County's R-6 zoning at the time of annexation. The existing property does meet required setbacks for all setbacks. The proposed addition meets all required setbacks as well.		
25' Required: Front Yard Setback	25' Proposed: Front Yard Setback (Existing)	
7.5' Required: Side Yard Setback	12' Proposed: Front Yard Setback (Existing)	
7.5' Required: Side Yard Setback	23' Proposed: Front Yard Setback (Existing)	
25' Required: Rear Yard Setback	48' Proposed: Rear Yard Setback	
Architectural Compatibility (5/A & 5/R):		
The proposed architecture and finishes match the existing home and that of the other homes in the neighborhood.		
Exterior Materials and Colors:		
See Color/Material Board Metal Roof: Standing seam in "Vermont Slate" Shingle Roof: Asphalt Shingles to match existing Window/ Wood Trim: Clad to match existing, painted/stained in "Gray Matters" Siding: vertical wood siding, Stained to match existing Stone Veneer: Match existing Trim and Fascia: Painted/Stained to match existing Accent: Corrugated Metal Siding, matte finish silver allowed to rust over time Front Porch: New metal railing with metal balusters The proposed material usage falls within the 25 percent or less threshold for non-natural materials on each facade.		
Roof:		
Existing shingle roof, proposed standing seam in "Vermont Slate"		
Garage Doors:		
Existing wood clad garage doors		
Landscaping (22A/22R): Staff finds that sufficient mature spruce and aspen trees exist to provide screening/buffering from the adjacent streets and neighboring lots. No new landscaping is proposed with this application.		
Defensible Space (22A):		
Complies		
Drainage (27A/27R):		
Positive drainage away from the residence is proposed.		
Driveway Slope:		
12.00%		
Point Analysis (Sec. 9-1-17-3):		
This application has met all Absolute Policies. Staff has not awarded nor deducted any points for this proposal.		
Staff Action:		
Staff approves the Engle Residence Addition, PL-2023-0044, Lot 8, Block 1, Warrior's Mark West #5, with a passing point analysis of zero (0) points, located at 102 New England Drive, with the attached Findings and Conditions.		
Additional Findings and Conditions of Approval:		
6. Required setbacks for the Warrior's Mark West Filing #5 are the same as what was specified under Summit County's R-6 zoning at the time of annexation. The existing property does meet all required setbacks. The proposed addition meets all required setbacks as well. 13. The proposed railing and balusters for the entry porch on the front of the home shall be revised to a matte color that matches the existing structure and color palette.		

TOWN OF BRECKENRIDGE

**Engle Residence Addition
Lot 8, Block 1, Warriors Mark West #5
102 New England Drive
PL-2023-0044**

STAFF RECOMMENDATION: Staff has approved this application with the following Findings and Conditions and recommends the Planning Commission uphold this decision.

FINDINGS

1. The project is in accord with the Development Code and does not propose a prohibited use.
2. The project will not have significant adverse environmental impact or demonstrative negative aesthetic effect.
3. All feasible measures mitigating adverse environmental impacts have been included, and there are no economically feasible alternatives, which would have less adverse environmental impact.
4. This approval is based on the staff report dated **March 17th, 2023**, and findings made by the Planning Commission with respect to the project. Your project was approved based on the proposed design of the project and your acceptance of these terms and conditions imposed.
5. The terms of approval include any representations made by you or your representatives in any writing or plans submitted to the Town of Breckenridge, and at the hearing on the project held on **March 21st, 2023**, as to the nature of the project. In addition to Commission minutes, the audio of the meetings of the Commission are recorded.
6. Required setbacks for the Warrior's Mark West Filing #5 are the same as what was specified under Summit County's R-6 zoning at the time of annexation. The existing property does meet required setbacks for all setbacks. The proposed addition meets all required setbacks as well.

CONDITIONS

1. This permit does not become effective, and the project may not be commenced, unless and until the applicant accepts the preceding findings and following conditions in writing and transmits the acceptance to the Town of Breckenridge.
2. If the terms and conditions of the approval are violated, the Town, in addition to criminal and civil judicial proceedings, may, if appropriate, issue a stop order requiring the cessation of work, revoke this permit, require removal of any improvements made in reliance upon this permit with costs to constitute a lien on the property and/or restoration of the property.
3. This permit expires eighteen (18) months from date of issuance, on **September 28th, 2024**, unless a building permit has been issued and substantial construction pursuant thereto has taken place. In addition, if this permit is not signed and returned to the Town within 30 days from the permit mailing date, the duration of the permit shall be 18 months, but without the benefit of any vested property right.
4. The terms and conditions of this permit are in compliance with the statements of the staff and applicant made on the evidentiary forms and policy analysis forms.
5. Nothing in this permit shall constitute an agreement by the Town of Breckenridge to issue a certificate of occupancy for the project covered by this permit. The determination of whether a certificate of occupancy should be issued for such project shall be made by the Town in accordance with the applicable provisions of the Town Code, including, but not limited to the building code.

6. Driveway culverts shall be 18-inch heavy-duty corrugated polyethylene pipe with flared end sections and a minimum of 12 inches of cover over the pipe. Applicant shall be responsible for any grading necessary to allow the drainage ditch to flow unobstructed to and from the culvert.
7. At the point where the driveway opening ties into the road, the driveway shall continue for five feet at the same cross slope grade as the road before sloping to the residence. This is to prevent snowplow equipment from damaging the new driveway pavement.
8. Applicant shall field locate utility service lines to avoid existing trees.
9. An improvement location certificate of the height of the top of the foundation wall and the height of the building's ridge must be submitted and approved by the Town during the various phases of construction. The final building height shall not exceed 35' at any location.
10. At no time shall site disturbance extend beyond the limits of the platted building/site disturbance envelope, including building excavation, and access for equipment necessary to construct the residence.
11. All hazardous materials used in construction of the improvements authorized by this permit shall be disposed of properly off site.
12. Each structure which is authorized to be developed pursuant to this permit shall be deemed to be a separate phase of the development. In order for the vested property rights associated with this permit to be extended pursuant to Section 9-1-17-11(D) of the Breckenridge Development Code, substantial construction must be achieved for each structure within the vested right period of this permit.
13. 13. The proposed railing and balusters for the entry porch on the front of the home shall be revised to a matte color that matches the existing structure and color palette.

PRIOR TO ISSUANCE OF BUILDING PERMIT

14. Applicant shall submit proof of ownership of the project site.
15. Applicant shall submit and obtain approval from the Town Engineer of final drainage, grading, utility, and erosion control plans.
16. Applicant shall provide plans stamped by a registered professional engineer licensed in Colorado, to the Town Engineer for all retaining walls over four feet in height.
17. Any exposed foundation wall in excess of 12 inches shall be finished (i.e. textured or painted) in accordance with the Breckenridge Development Code Section 9-1-19-5R.
18. Applicant shall identify all existing trees, which are specified on the site plan to be retained, by erecting temporary fence barriers around the trees to prevent unnecessary root compaction during construction. Construction disturbance shall not occur beyond the fence barriers, and dirt and construction materials or debris shall not be placed on the fencing. The temporary fence barriers are to remain in place until issuance of the Certificate of Occupancy.
19. Existing trees designated on the site plan for preservation which die due to site disturbance and/or construction activities will be required to be replaced at staff discretion with equivalent new trees, i.e. loss of a 12 inch diameter tree flagged for retention will be offset with the addition of four 3-inch diameter new trees.
20. Applicant shall submit and obtain approval from the Town of a construction staging plan indicating the location and type of construction fencing, all construction material storage, fill and excavation material storage areas, portolet and dumpster locations, and employee vehicle parking areas. No staging is permitted within public right of way without Town permission. Any dirt tracked upon the public road shall be the applicant's

responsibility to remove. Contractor parking within the public right of way is not permitted without the express permission of the Town, and cars must be moved for snow removal. A project contact person is to be selected and the name provided to the Public Works Department prior to issuance of the building permit.

21. The public access to the lot shall have an all weather surface, drainage facilities, and all utilities installed acceptable to Town Engineer. Fire protection shall be available to the building site by extension of the Town's water system, including hydrants, prior to any construction with wood. In the event the water system is installed, but not functional, the Fire Marshall may allow wood construction with temporary facilities, subject to approval.
22. Applicant shall submit and obtain approval from Town staff of a cut sheet detail for all exterior lighting on the site. All exterior lighting on the site or buildings shall be fully shielded to hide the light source and shall cast light downward. Exterior residential lighting shall not exceed 15' in height from finished grade or 7' above upper decks.

PRIOR TO ISSUANCE OF CERTIFICATE OF OCCUPANCY

23. Applicant shall revegetate all disturbed areas with a minimum of 2 inches topsoil, seed and mulch.
24. Applicant shall remove leaf clutter, dead branches and dead standing trees from the property, dead branches on living trees shall be trimmed to a minimum height of six (6) feet and a maximum height of ten (10) feet above the ground.
25. Applicant shall remove all vegetation and combustible material from under all eaves and decks.
26. Applicant shall create defensible space around all structures as required in Policy 22 (Absolute) Landscaping.
27. Applicant shall paint all garage doors, metal flashing, vents, flues, rooftop mechanical equipment, meters, and utility boxes on the building a flat, dark color or to match the building color.
28. All exterior lighting on the site or buildings shall be fully shielded to hide the light source and shall cast light downward. Exterior residential lighting shall not exceed 15 feet in height from finished grade or 7 feet above upper decks.
29. At all times during the course of the work on the development authorized by this permit, the permittee shall refrain from depositing any dirt, mud, sand, gravel, rubbish, trash, wastepaper, garbage, construction material, or any other waste material of any kind upon the public street(s) adjacent to the construction site. Town shall provide oral notification to permittee if Town believes that permittee has violated this condition. If permittee fails to clean up any material deposited on the street(s) in violation of this condition within 24 hours of oral notice from Town, permittee agrees that the Town may clean up such material without further notice and permittee agrees to reimburse the Town for the costs incurred by the Town in cleaning the streets. Town shall be required to give notice to permittee of a violation of this condition only once during the term of this permit.
30. The development project approved by this Permit must be constructed in accordance with the plans and specifications, which were approved by the Town in connection with the Development Permit application. Any material deviation from the approved plans and specifications without Town approval as a modification may result in the Town issuing a Stop Work Order and/or not issuing a Certificate of Occupancy or Compliance for the project, and/or other appropriate legal action under the Town's development regulations. A Stop Work Order may not be released until a modification to the permit is reviewed and approved by the Town. Based upon the magnitude of the modification, another hearing before the Planning Commission may be required.
31. No Certificate of Occupancy or Certificate of Compliance will be issued by the Town until: (i) all work done pursuant to this permit is determined by the Town to be in compliance with the approved plans and specifications for the project, and all applicable Town codes, ordinances and standards, and (ii) all conditions of approval set forth in the Development Permit for this project have been properly satisfied. If either of these requirements cannot be met due to prevailing weather conditions, the Town may issue a Certificate of

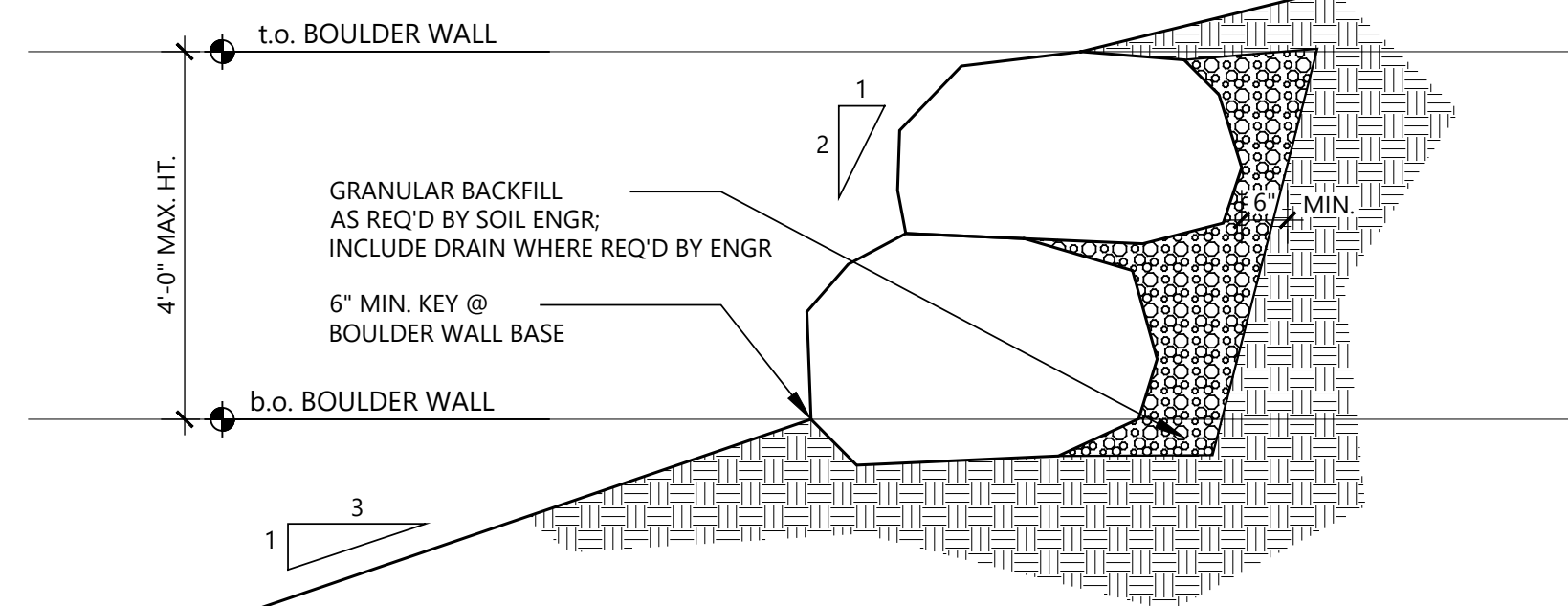
Occupancy or Certificate of Compliance if the permittee enters into a Cash Deposit Agreement providing that the permittee will deposit with the Town a cash bond, or other acceptable surety, equal to at least 125% of the estimated cost of completing any required work or any applicable condition of approval, and establishing the deadline for the completion of such work or the satisfaction of the condition of approval. The form of the Cash Deposit Agreement shall be subject to approval of the Town Attorney. "Prevailing weather conditions" generally means that work can not be done due to excessive snow and/or frozen ground. **As a general rule, a cash bond or other acceptable surety will only be accepted by the Town between November 1 and May 31 of the following year. The final decision to accept a bond as a guarantee will be made by the Town of Breckenridge.**

32. Applicant shall submit the written statement concerning contractors, subcontractors and material suppliers required in accordance with Ordinance No. 1, Series 2004.
33. **Applicant shall be held responsible for any deterioration or damages caused by development or construction activities to any Town infrastructure, public rights-of-way, or public property. This includes but is not limited to deterioration or damages to roadway surfaces, curbs, drainage systems, sidewalks, and signage. Applicant must rectify such deterioration or damages to the previous condition at their own expense. Town shall provide written notification to permittee if Town believes that permittee has caused deterioration or damages which would enact this condition. If permittee fails to rectify deterioration or damages in violation of this condition, permittee agrees that the Town may resolve such deterioration or damages and permittee agrees to reimburse the Town for the costs incurred by the Town. Town shall be required to give notice to permittee of a violation of this condition only once during the term of this permit. Any failure to rectify deterioration or damages or provide reimbursement without Town approval may also result in the Town issuing a Stop Work Order and/or not issuing a Certificate of Occupancy or Compliance for the project, and/or other appropriate legal action under the Town's development regulations.**
34. The development authorized by this Development Permit may be subject to the development impact fee imposed by Resolution 2006-05 of the Summit County Housing Authority. Such resolution implements the impact fee approved by the electors at the general election held November 7, 2006. Pursuant to intergovernmental agreement among the members of the Summit Combined Housing Authority, the Town of Breckenridge is authorized to administer and collect any impact fee which is due in connection with development occurring within the Town. For this purpose, the Town has issued administrative rules and regulations which govern the Town's administration and collection of the impact fee. ***Applicant will pay any required impact fee for the development authorized by this Development Permit prior to the issuance of a Certificate of Occupancy.***

(Initial Here)

MAX. BOULDER SIZE:
WALLS LESS THAN 2' TALL -
18" BOULDER DIM.
WALLS MORE THAN 2' TALL -
36" BOULDER DIM.

MIN. BOULDER SIZE:
TOP HALF OF WALL -
12" BOULDER DIM.
BOTTOM HALF OF WALL -
18" BOULDER DIM.



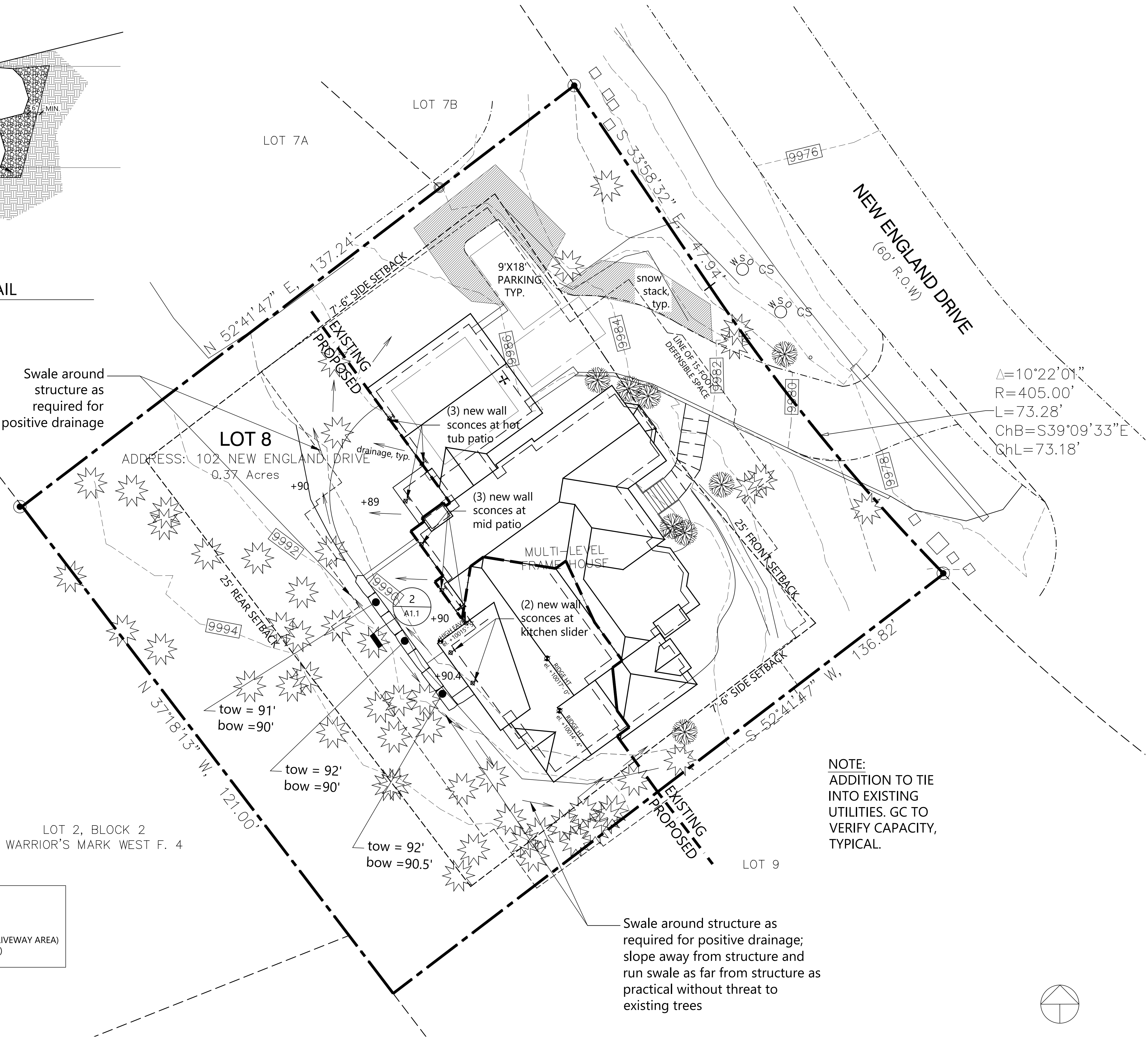
NOTE:
ALL BOULDER WALLS OVER
4'-0" TALL WILL BE DESIGNED BY
AN ENGINEER REGISTERED IN THE
STATE OF COLORADO

2 BOULDER RETAINING WALL DETAIL
NTS

- LANDSCAPING NOTES:**
- All plant materials to be of native/indigenous species and origin with all trees to be native dug at 8500' or greater elevation.
 - Drip irrigation to be provided on all new plantings and revegetation.
 - All rocks (decorative) to match native stone found on site. All boulder retaining walls to match native stone or to be Sandstone Slab, re: detail.
 - Unless otherwise designated all roof edge drip-lines to receive a 3' wide path of cobble (2'-4" native or approved rock) splash area on weed barrier fabric to a depth adequate to cover fabric.
 - All disturbed areas not otherwise planted and existing disturbed areas to be planted w/ Summit County Grass Seed Mix. Most areas to receive occasional hand scatterings of individual native wildflower species (appropriate to the site) in plant community groupings before hydromulching.
 - All seeding applications to be hand spread, raked or dragged in and hydromulched down. No seeding is to be applied mixed with hydromulch alone.
 - All trees and shrubs to receive mulch rings of aspen mulch to a minimum depth of 3-4" and the diameter of the drip line for conifers, 3' for deciduous trees and 2' for shrubs.
 - Slopes of greater than 3:1 shall have straw netting installed per manufacturers specification.

SITE AREAS:

LOT AREA	16,328 SF
BUILDING/NON-PERMEABLE	4,320 SF
DRIVEWAY AREA	1,232 SF
SNOW STACK	548 SF (44.4% DRIVEWAY AREA)
OPEN/PERMEABLE	10,776 SF (66 % LOT)



NOTE:
ADDITION TO TIE
INTO EXISTING
UTILITIES. GC TO
VERIFY CAPACITY,
TYPICAL.

Swale around structure as
required for positive drainage;
slope away from structure and
run swale as far from structure as
practical without threat to
existing trees

1 SITE PLAN
1" = 10'-0"

Revisions:

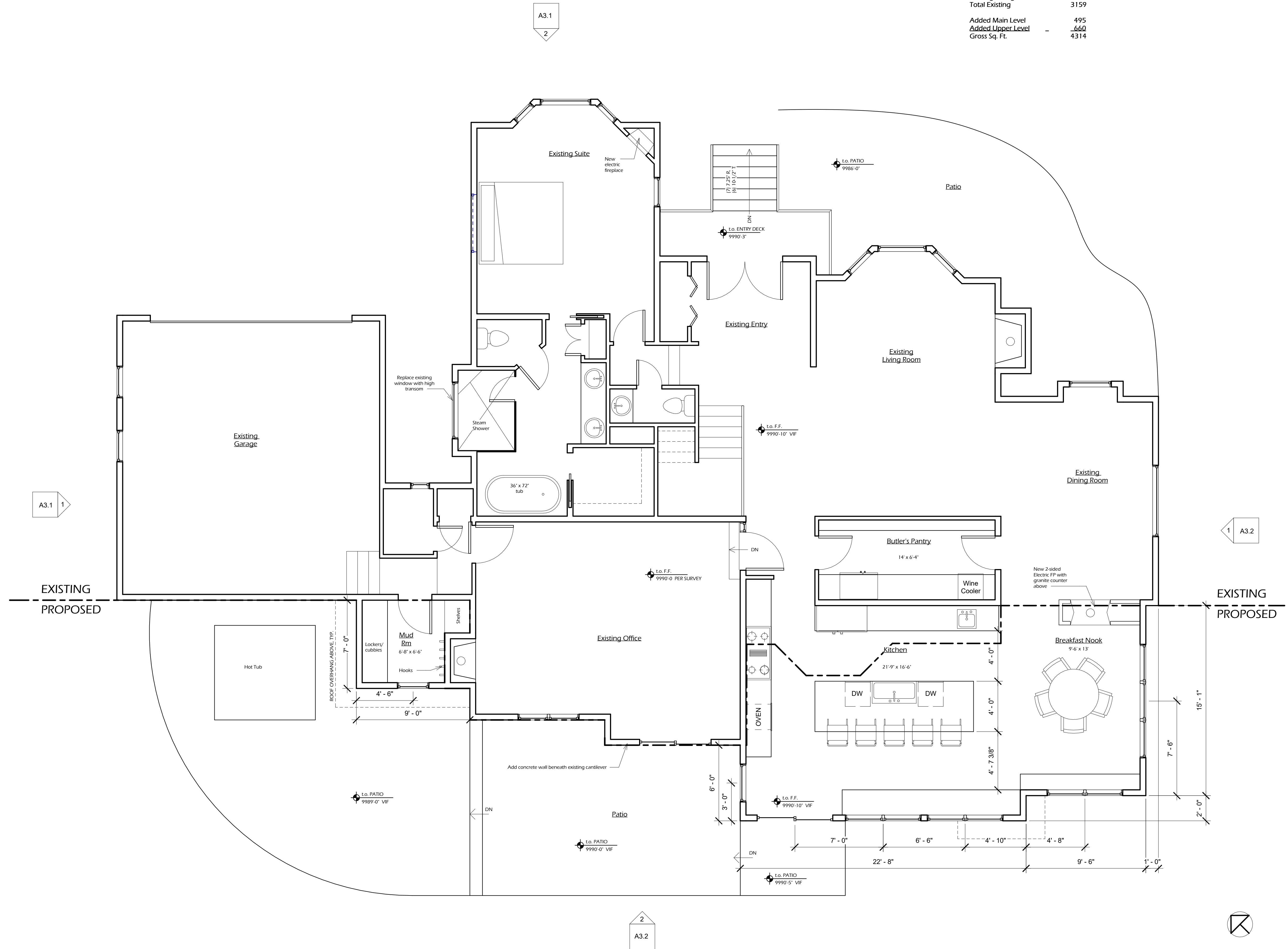
Issue Date:

03/06/23
02/24/23
02/15/23
02/13/23

Title:
Site Plan

Sheet:
A1.1
Project No:
22212

Square Footage:	
Existing Main Level	1856
Existing Upper Level	841
Existing Garage	462
Total Existing	3159
Added Main Level	495
Added Upper Level	460
Gross Sq. Ft.	4314



1 Main Level
 1/4" = 1'-0"

Engle Addition
 Lot 8 Block 1 Warrior's Mark
 West Filing #5, Breckenridge,
 CO

Revisions:

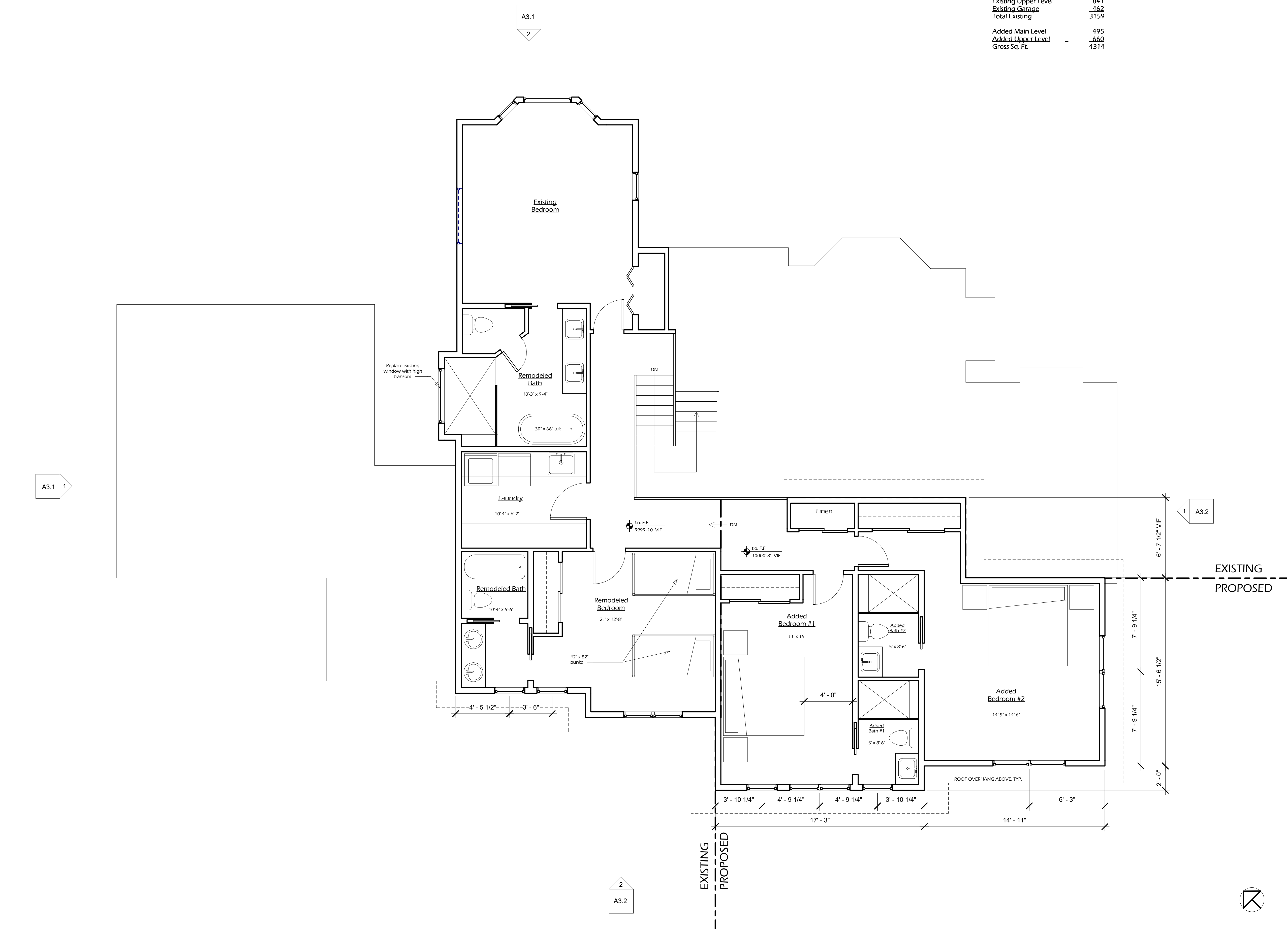
Issue Date:

02/24/23
02/15/23
02/13/23
01/24/23

Title:
Main Level Plan

Sheet:
A2.1
 Project No:
 22212

Square Footage:	
Existing Main Level	1856
Existing Upper Level	841
Existing Garage	462
Total Existing	3159
Added Main Level	495
Added Upper Level	660
Gross Sq. Ft.	4314



1 Upper Level
1/4" = 1'-0"

Revisions:

Issue Date:

02/24/23

02/15/23

02/13/23

01/24/23

Title:

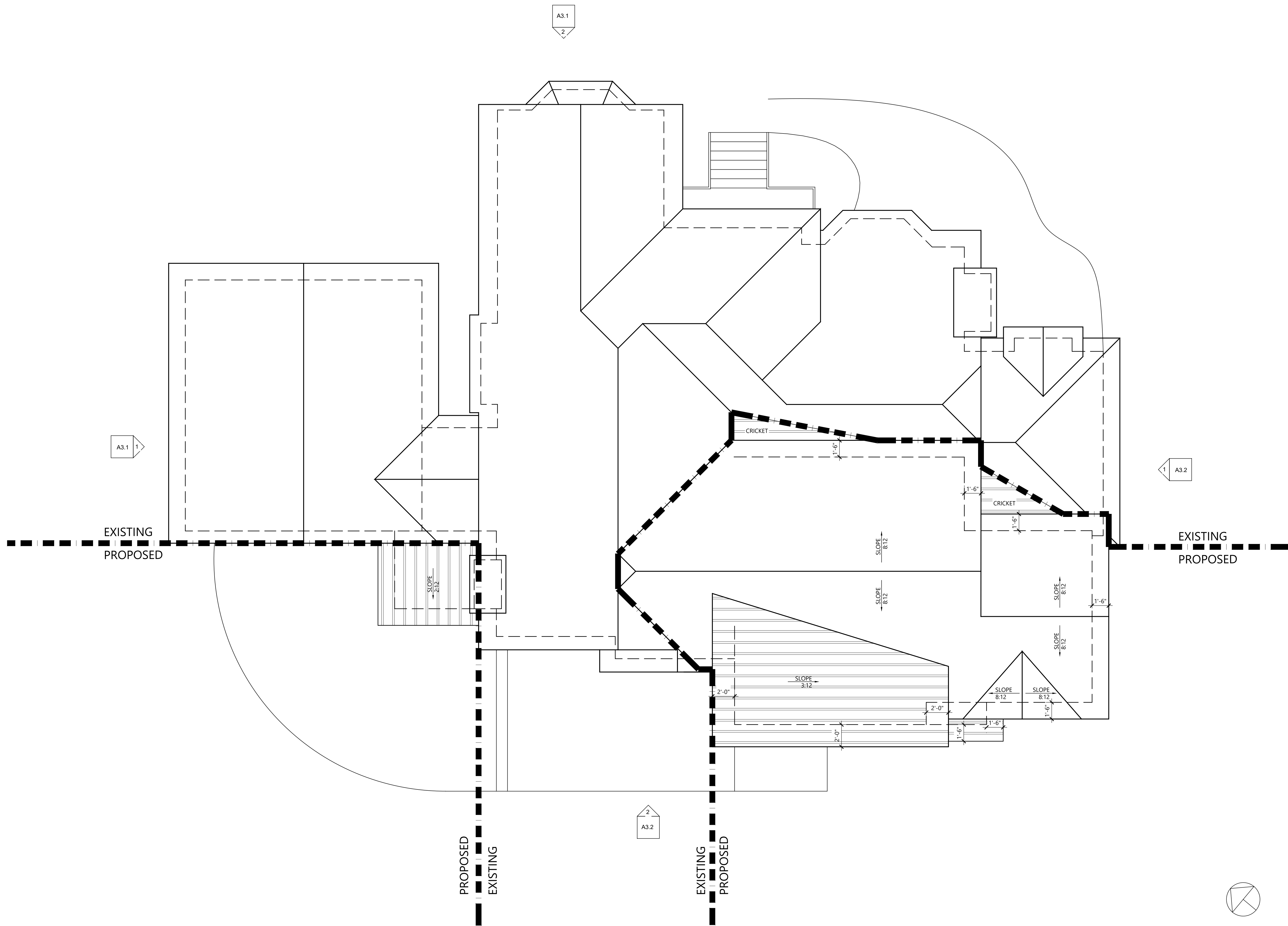
Upper
Level Plan

Sheet:

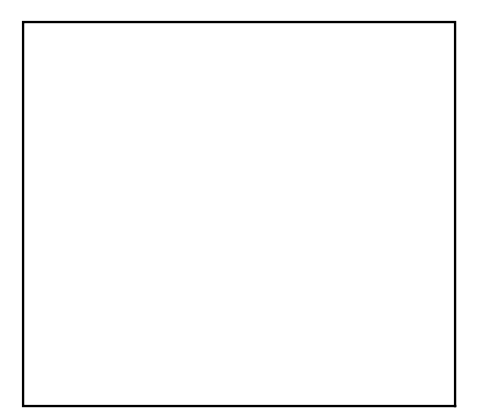
A2.2

Project No:

22212



1 ROOF PLAN
1/4"=1'-0"



Engle Addition
Lot 8 Block 1, Warrior's Mark West
Filing #5
Breckenridge, Colorado

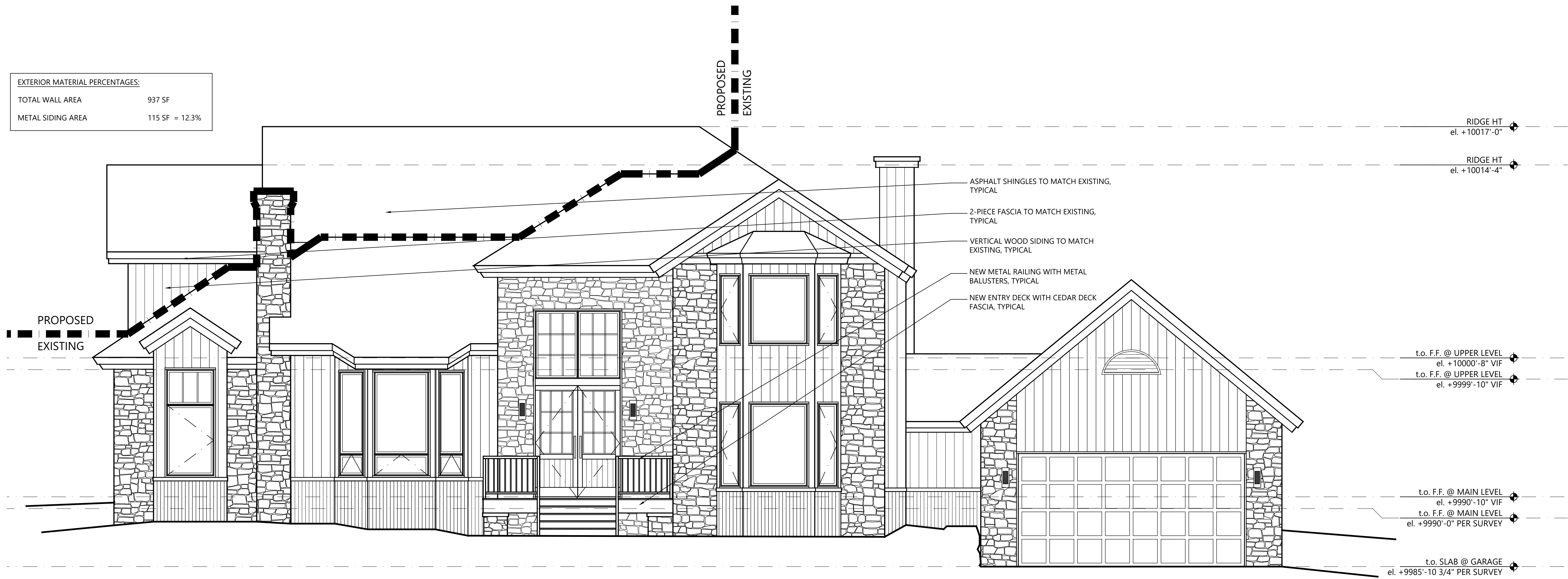
Revisions:

Issue Date:
02/24/23

Title:
Roof Plan

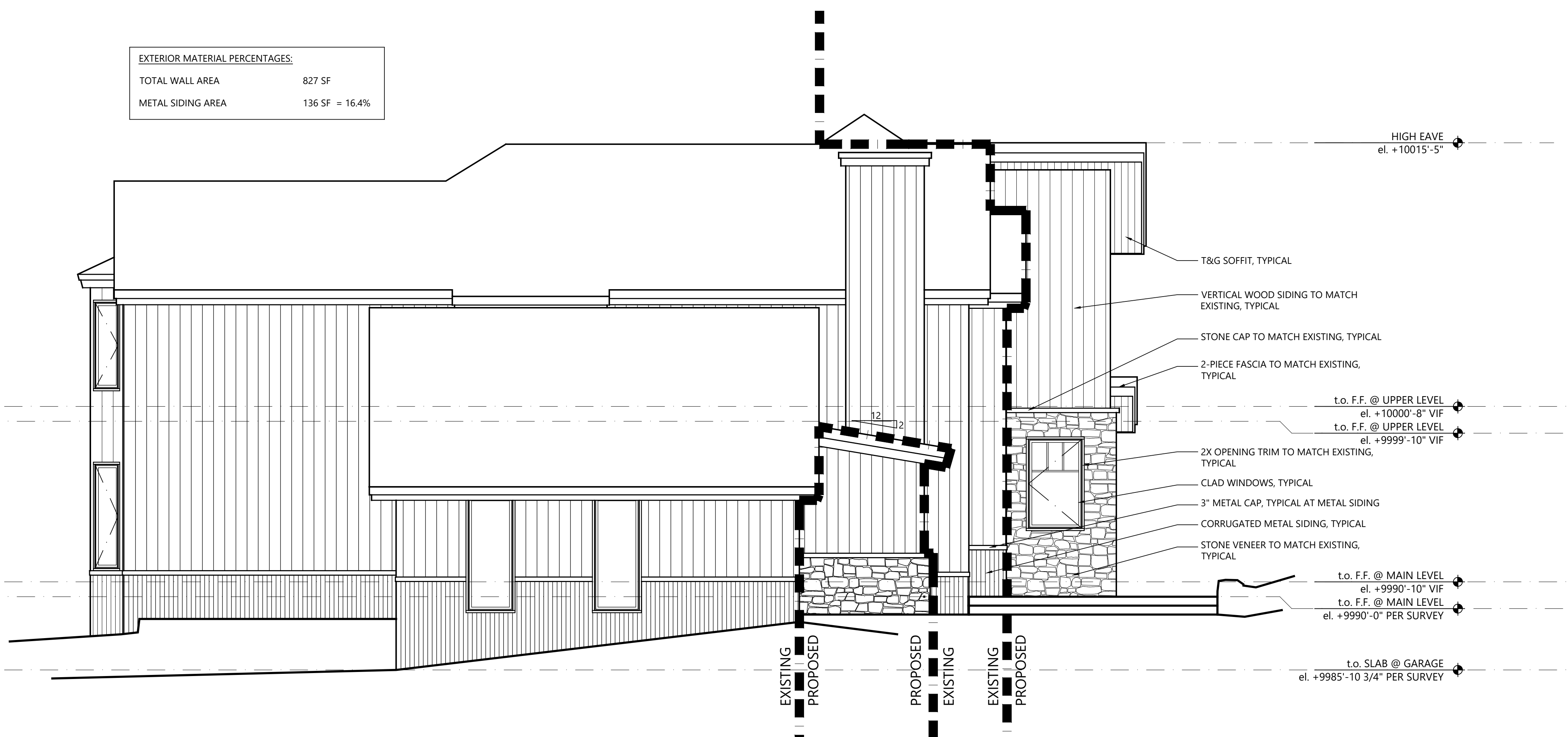
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A2.3
Project No:
22212

EXTERIOR MATERIAL PERCENTAGES:	
TOTAL WALL AREA	937 SF
METAL SIDING AREA	115 SF = 12.3%



2 NORTHEAST ELEVATION
1/4"=1'-0"

EXTERIOR MATERIAL PERCENTAGES:	
TOTAL WALL AREA	827 SF
METAL SIDING AREA	136 SF = 16.4%



1 NORTHWEST ELEVATION
1/4"=1'-0"

Revisions:	

Issue Date:	
03/06/23	
02/24/23	
02/15/23	
02/13/23	

Title:
Building Elevations

Sheet:
A3.1
Project No:
22212

Revisions:

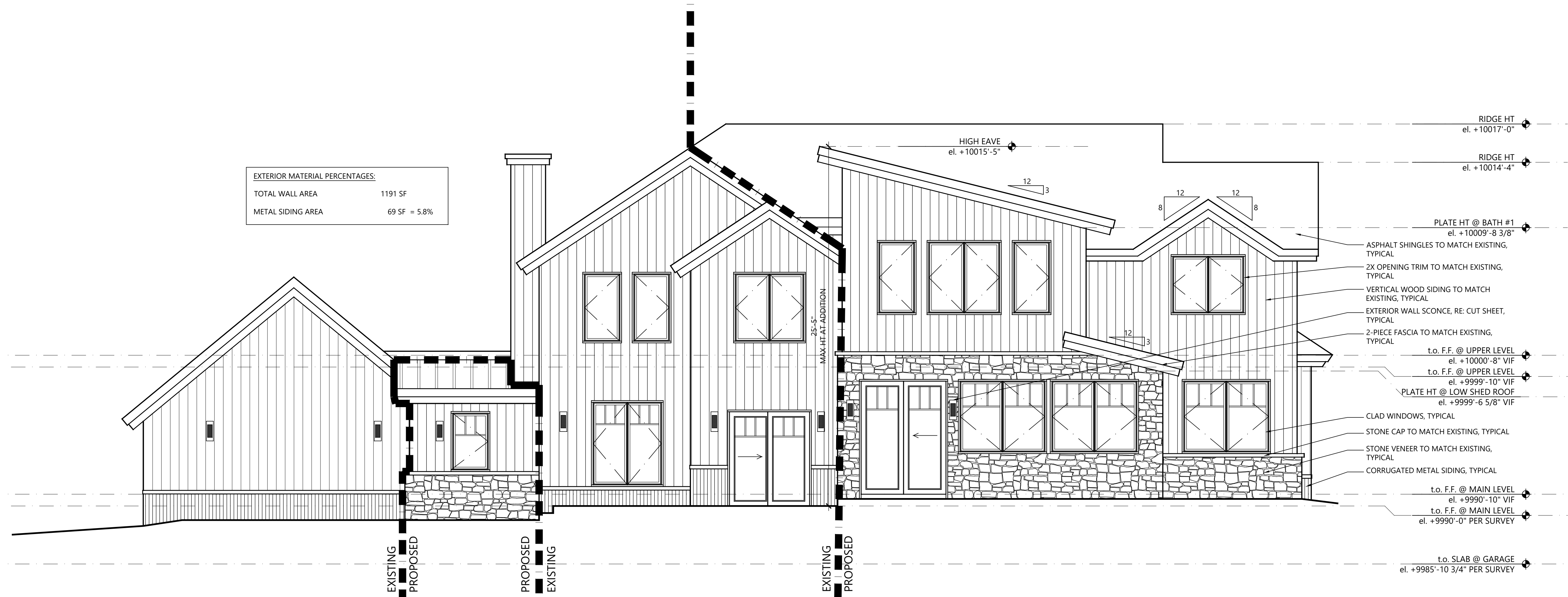
Issue Date:

03/06/23
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02/15/23
02/13/23

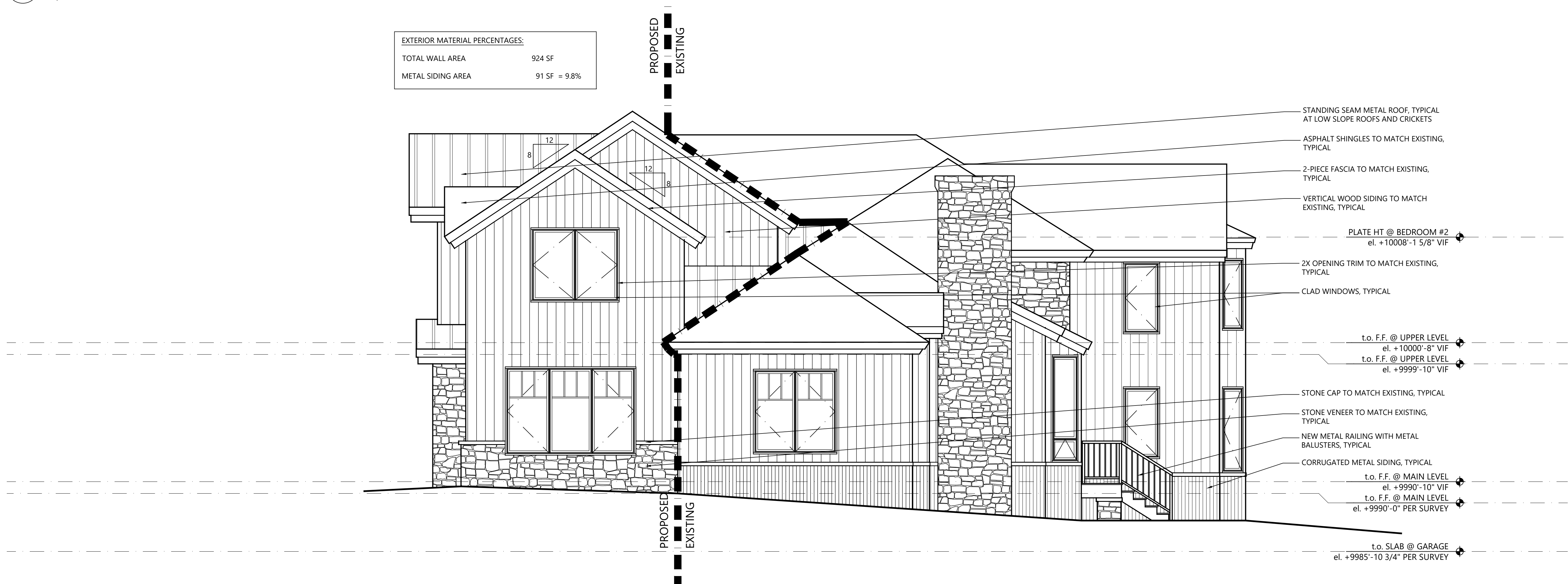
Title:
Building Elevations

Sheet:
A3.2
 Project No:
 22212

2 **SOUTHWEST ELEVATION**
 1/4"=1'-0"



1 **SOUTHEAST ELEVATION**
 1/4"=1'-0"



Engle Addition

METAL ROOF:
STANDING SEAM IN
"VERMONT SLATE"

SHINGLE ROOF:
ASPHALT SHINGLES
TO MATCH EXISTING

FASCIA:
PAINTED/STAINED
TO MATCH EXISTING



SIDING:
STAINED TO
MATCH EXISTING

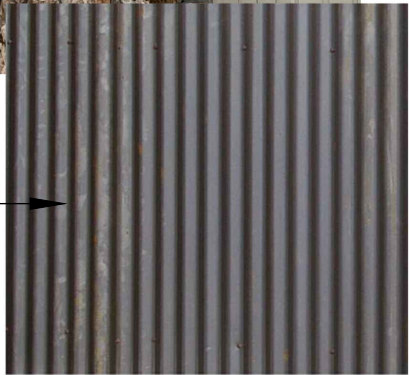
WINDOWS:
CLAD TO MATCH EXISTING

STONE VENEER:
MATCH EXISTING

WOOD TRIM:
PAINTED/STAINED TO
MATCH EXISTING



CORRUGATED METAL
SIDING:
MATTE FINISH SILVER
ALLOWED TO RUST OVER
TIME



COLOR SAMPLE BOARD

Lot 8 Block 1, Warrior's Mark West Filing #5

Home / outdoor lighting / wall light / dark sky / Style # 2V223



Hinkley Luna 16 3/4" High LED Black Outdoor Wall Light

\$459.00

or 4 interest-free payments of **\$114.75** with sezzle [Ⓢ]

FREE SHIPPING & **FREE RETURNS*** | Low Price Guarantee

In Stock - Ships in 3 to 5 Days

1

ADD TO CART

SAVE

[Check Store Availability](#) | [Question? Ask a Store Associate](#)

Good To Know



DESIGN CHAT VIEW IN YOUR ROOM

Product Details

Place this chic modern outdoor LED wall light next to an exterior door.

Additional Info:

This contemporary outdoor wall light features satin black finish and etched glass lenses. An open cylinder design in metal includes dual LED modules that illuminate from the top and bottom of the fixture, enhancing the stylish minimal feel. ADA and Dark Sky-compliant. A chic and energy-saving addition to your exterior decor from Hinkley outdoor lighting.

HINKLEY



[Shop all Hinkley](#)

- Contemporary cylinder outdoor wall light
- Satin black finish.
- Metal construction.
- Etched glass lenses.
- From the Hinkley Lighting collection.
- Includes two 5.5 watt LED modules (11 watts total).
- Light output is 500 lumens, comparable to a 40 watt incandescent light.
- Color temperature is 2700K.
- CRI 90.
- Dimmable using an electronic low voltage (ELV) dimmer, or an LED-rated dimmer.
- ADA and Dark Sky compliant.
- California Title 24 compliant.
- Measures 16 3/4" high, 6" wide.

- Extends 3 1/2" from the wall.
- Backplate is 16" by 6".
- Mounting point to top of fixture is 10" to 13 1/2".
- Wet location outdoor rated.

SPECIFICATIONS

PRODUCT ATTRIBUTES

Finish	Black
Style	Mid-Century
Brand	Hinkley
Dark Sky	Yes
LED	Yes

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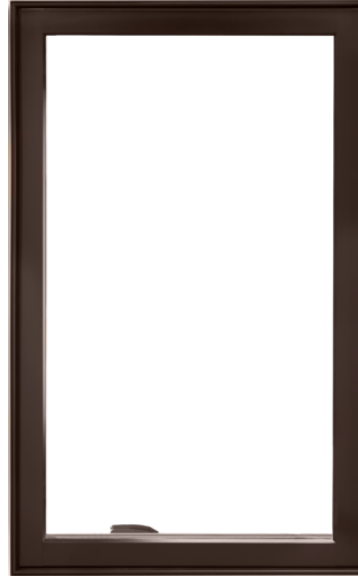


CASEMENT
SIGNATURE SERIES

Honoring classic design aesthetics, this aluminum-clad wood collection makes Weather Shield quality accessible across a broad range of popular home styles.

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SIGNATURE SERIES CASEMENT

With a unique design that features a side-hinge and a hand crank to open, our casement windows offer the perfect blend of aesthetic appeal and ease of use. Crafted using our Tricore™ Frame Technology, casement windows provide exceptional energy efficiency and come in finish options to perfect the look of your home.

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Style

Tricore™ Frame Technology features extruded aluminum exterior cladding that's easy to maintain and stands up to the harshest elements.



Configuration

Available in a variety of styles including bows, bays, picture combination windows and more.

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Low Maintenance

Durable and virtually maintenance free, extruded aluminum exteriors stand up to the harshest elements, resisting dents, dings, and scratches.



Operation

Casement windows are hinged from the side and open easily with a crank.

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Materials

The all-wood interior sash offers incredible design flexibility and can be prefinished with one of ten factory finishes for total customization.



Energy Efficiency

Choices including energy-efficient Zo-e-shield® glazing and ENERGY STAR® rated windows.

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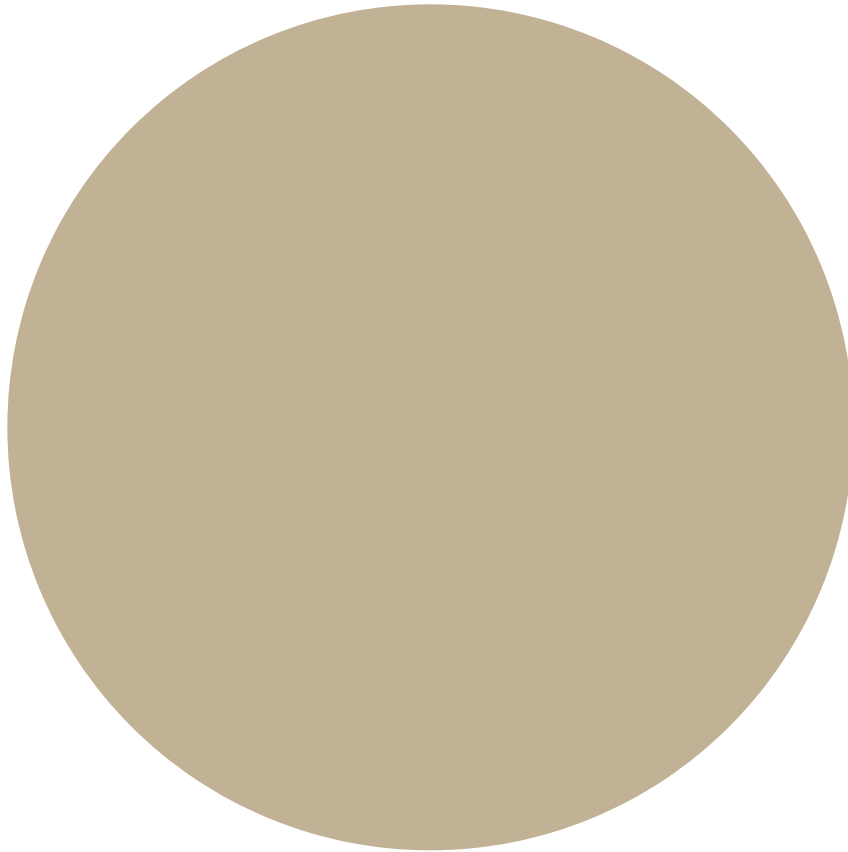
To help you get the exact look you desire, we offer an ever expanding palette of durable and distinctive exterior color options rated to the highest industry standards for color retention. Finishes and colors shown may not be actual representations. Please visit authorized Weather Shield dealer for color samples.

STANDARD ALUMINUM FINISHES

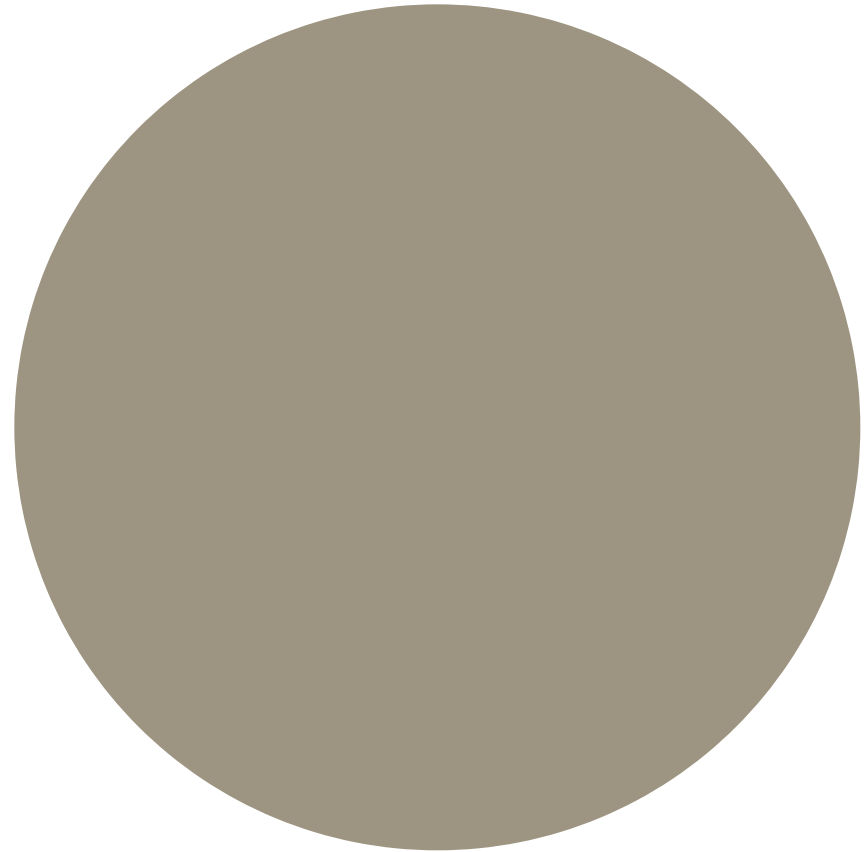


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TAN

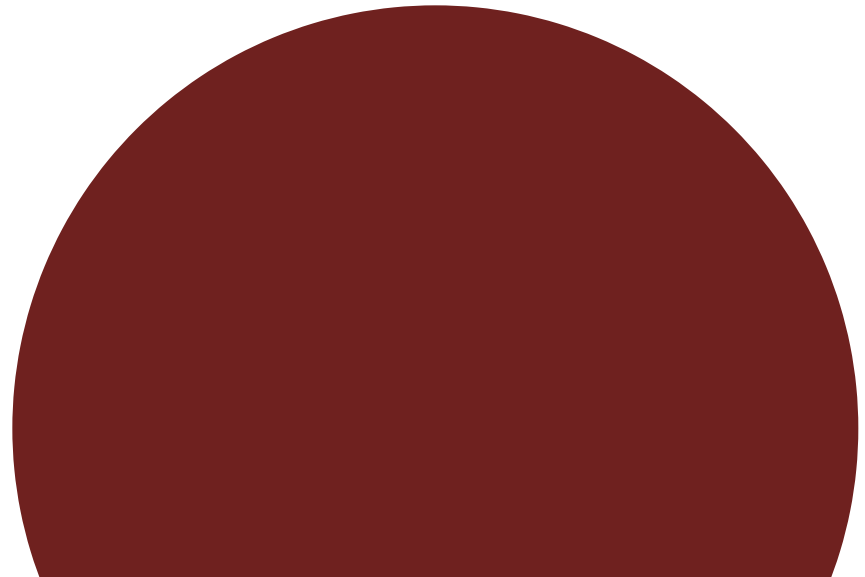


DESERT TAN



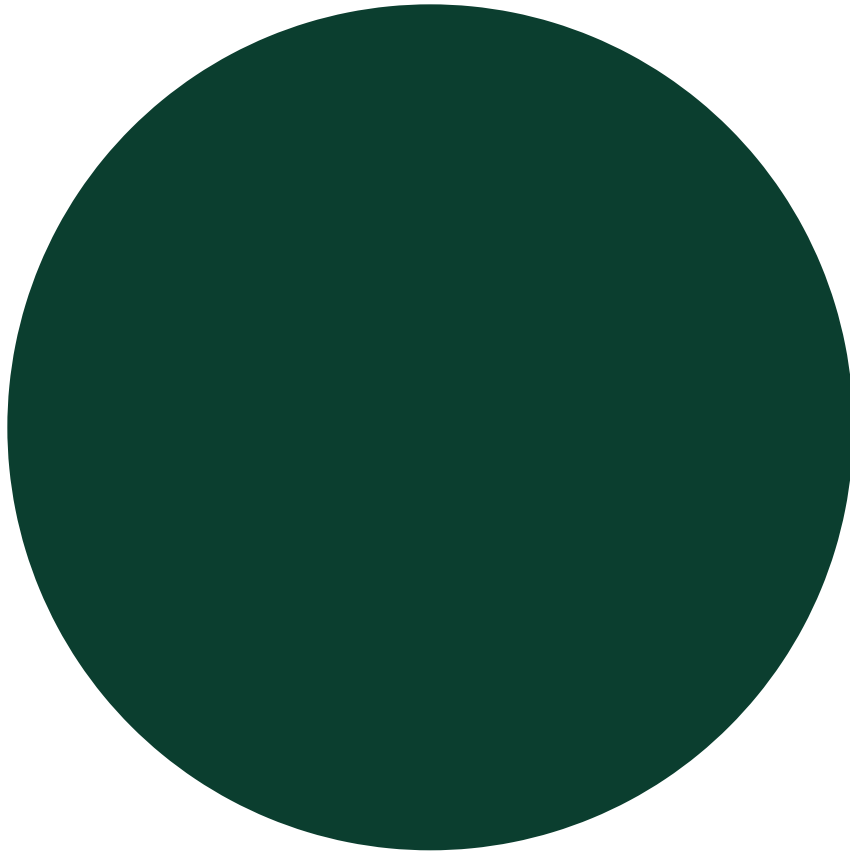
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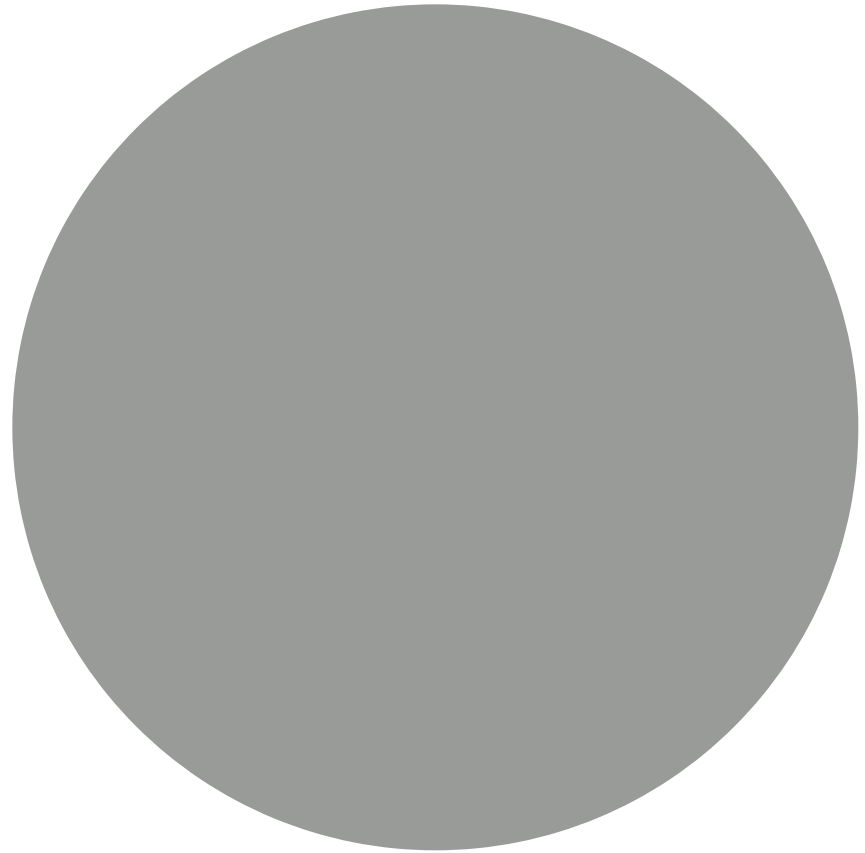


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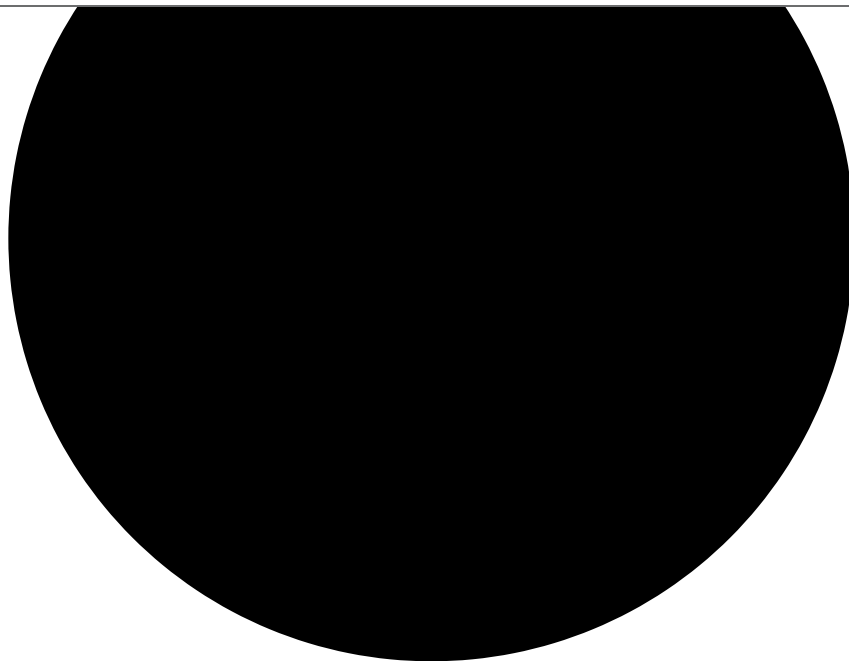


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Exterior trim allows for a clean and finished exterior look.



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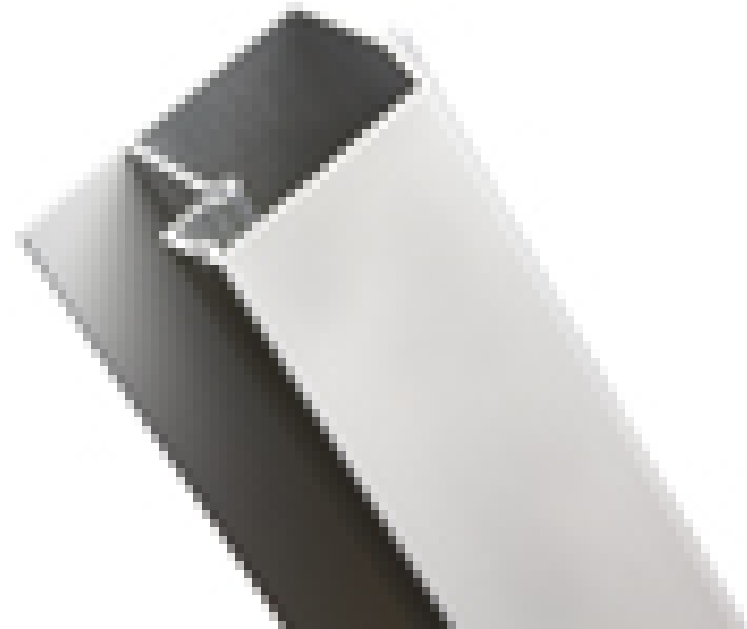


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1" BULL NOSE



2" BULL NOSE

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HARDWARE 

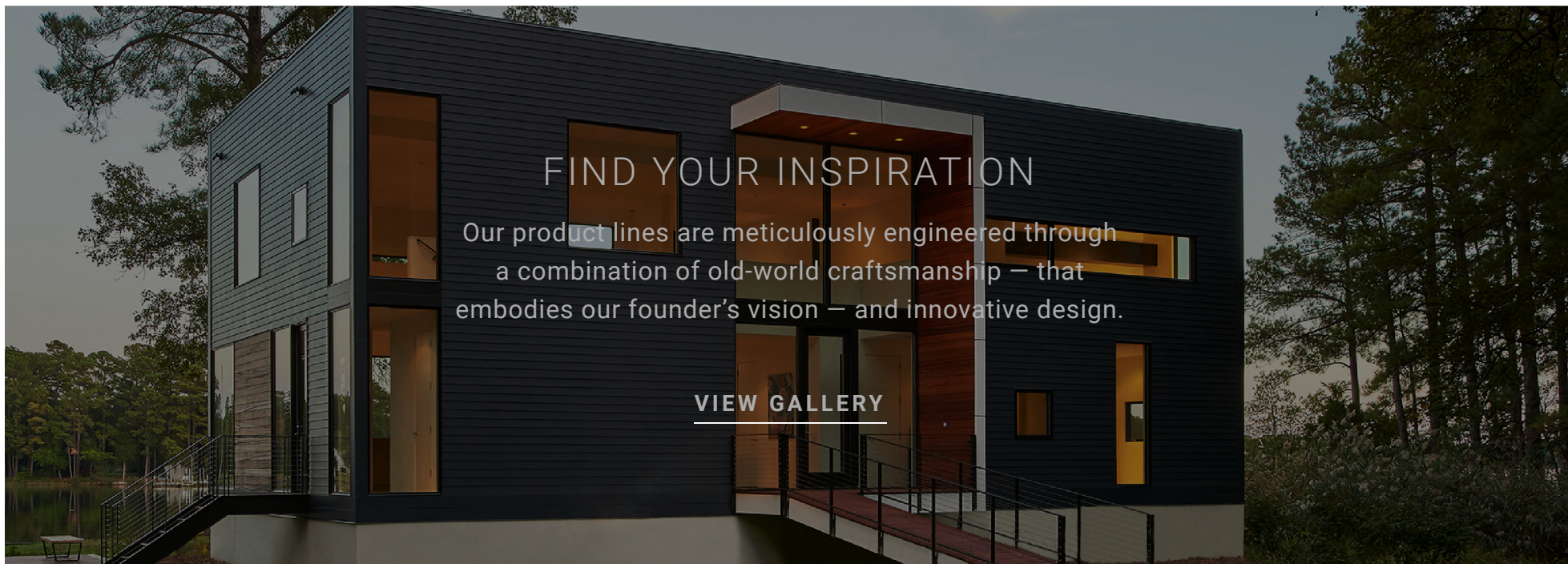
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Memo

To: Planning Commission
From: Julia Puester, AICP, Assistant Community Development Director
Date: 3/16/2023 for meeting of 3/28/23
Subject: Capacity Analysis Informational Item

The purpose of this work session is to inform the Planning Commission of the recently completed capacity analysis and dashboard, which is a new tool that the Town has to monitor trends in primary congestion/capacity indicators over time. The dashboard will be updated annually, reporting back to Town Council, and modified as appropriate in the future.

Background:

In 2022, the Town Council voiced concerns that Town seemed more congested and that we were experiencing more impacts to Town infrastructure. To address this, the Council directed staff to engage a study to determine if it was a perception or reality that Town was experiencing an increase in congestion and crowding in Town. The Town hired RRC Associates to answer the following questions:

1. Is Town seeing a significant increase in the number of people in the area over the past 5 years?
2. If so, what is the makeup and share of residents, commuters, and visitors contributing to this increase?
3. What does the data tell us regarding future projections of these trends? Which metrics are most useful for the Town to monitor and track year-over-year to better understand changes in visitation and growth over time?

Staff and RRC presented the capacity analysis and dashboard to the Town Council on January 24th. In addition to the study attached, staff will present the dashboard with primary indicators at the work session.

Although many data sources were explored, some were not utilized if data points were not readily available, found to be irrelevant, unreliable, or inconsistently collected. The primary indicators found to provide the most relevant available data to date and therefore used in the study include:

1. Mobile location data (cell phone data)
2. Traffic counts
3. Trail user counts
4. Sales tax collection
5. Lodging room nights and Average Daily Rate
6. School enrollment numbers

Other data not used, such as parking counts and transit numbers, are believed to be important for future monitoring. However, that data collection has been inconsistent (e.g. transit routes eliminated during covid, parking counted inconsistently). Staff is working on plans to methodically collect these data points going forward and a placeholder has been included in the dashboard.

Summary

The study points to an increase in visitation, not local residents, most recently from 2019 to 2021. The Highway 9/Tiger Road traffic counter and mobile location data showed an increase in visitor numbers. Sales tax, specifically related to short-term rentals, increased dramatically and in turn, sales tax overall increased. Trail usage was up and winter of 2021/2022 witnessed the highest average daily lodging room nights to date. While 2021 does appear to be the busiest we have seen, it may be an outlier that succumbed to pent up demand for travel due to COVID impacts. In comparison, 2022 saw lower numbers in some indicators and the Town may be returning to lower levels of visitation.

Town of Breckenridge 2022 Capacity Analysis



PREPARED BY:
RRC Associates

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PREPARED FOR:
Town of Breckenridge, CO



**TOWN OF
BRECKENRIDGE**

Executive Summary

The Town of Breckenridge 2022 Capacity Analysis uses a variety of data sources, both internal and external to the Town, to assess trends of use by and volume of different types of people in the area over time. The primary reasons for this study were to identify

- a) Whether the perception of increased crowding and congestion is a reality, and if so,
- b) If such increases are attributable to specific user groups (i.e., visitors vs. locals) and,
- c) To what extent these patterns can be analyzed and monitored.

At present, the analysis of all relevant data sources results in a variety of noteworthy trends. Sales tax collections are reaching all-time highs for Breckenridge in 2021 and 2022, despite the fear of a lack of spending due to COVID-19 and uncertain economic conditions. However, school enrollment and population statistics for Breckenridge show no or very modest increases. The traffic count data collected by CDOT along Highway 9 provides some of the most granular data available for monitoring overall congestion of vehicles. Calendar year 2021 saw extremely high numbers of vehicles on Highway 9, while summer 2022 has been down moderately from peaks seen last year. Overall, traffic counts are elevated when compared to prior years.

When taken together, all the best available data point to a very busy past few years in Breckenridge – especially 2021. While 2021 does seem to be somewhat of an anomaly, with a strong ‘over-correction’ from major travel and recreation disruptions in 2020, the area has most certainly seen an increasing trend when it comes to volume of use the past 5 years, with the outdoor recreation areas impacted most significantly.

The research team worked both with the Town and through its own external data review to identify the most relevant datasets for this project. While many datasets provided useful insight into the research questions at hand, they varied in granularity, regularity, and quality. Some, such as school enrollment data, may be useful to check in on periodically, but do not require regular monitoring. Others, such as parking data, could prove incredibly useful, but the current available data would require considerable cleaning and work to derive meaningful, reliable conclusions – and thus do not lend themselves readily to the goals and scope of this project.

As such, the highest priority datasets recommended for regular continual monitoring are:

- Traffic (vehicle) counts
- Trail user counts
- Sales tax collections
- Lodging room nights
- Mobile device geolocation data

- Parking counts (when consistently available)

While identifying the user groups to whom increased use is attributable may not change the outlook of the future, a better understanding of these trends does enable more accurate monitoring and management approaches. The present analysis does not indicate that changes in habits and/or volume of the local and regional Breckenridge population are a primary cause of increased crowding and congestion in the area. Rather, the Denver Front Range, commuters, and destination visitors all play a strong role. There is high demand for Breckenridge from outside the local area, both for employment and simply to enjoy all the area has to offer.

Introduction

In recent years, Breckenridge, like many other popular resort destinations, has faced emerging and increasing concerns related to crowding, congestion, and the competing balance between destination marketing and destination management. The Town (led by the Breckenridge Tourism Office) conducted Resident Sentiment Surveys in 2017, 2019, and 2021 addressing these topics.



Results from these surveys have continually indicated that Breckenridge's residents, second homeowners, and business owners value the benefits that tourism brings to the community but seek to manage and reduce the negative impacts. Tourism is necessary for Breckenridge to survive, but it is important to also manage. While there are strong opinions on both ends of the spectrum, results have been consistent over time, even during the COVID-19 pandemic.

While resident sentiment toward tourism has remained consistent over time, the pressure of these concerns is mounting as the travel and tourism industry recovers and expands. Outdoor recreation travel, particularly in the Western U.S., exploded in 2021. Many destinations have received unprecedented pressure on infrastructure while also experiencing a significant shortage of workforce, especially in the hospitality and tourism industries. Understanding how visitation and movement patterns of people to and within the Breckenridge area have changed is critical when it comes to keeping a pulse on management of both local tourism and, where possible, long-range regional planning.

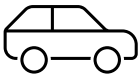



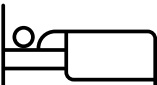

In Spring of 2022, the Town of Breckenridge contracted RRC, a market research and social science firm, to build an internal facing data dashboard and accompanying analysis that address the capacity and use of residents and visitors into and out of Breckenridge. The purpose of this analysis was to address the following research questions:

- 1) Is Breckenridge seeing a significant increase in the number of people in the area over the past 5+ years?
- 2) If so, what is the makeup and share of residents, commuters, and visitors contributing to this increase?
- 3) What does the best available data tell us regarding future projections of these trends? Which metrics are most useful for the Town to monitor and track year-over-year to better understand changes in visitation and growth over time?

Data Sources

To answer the primary research questions, the research team both worked with the Town and utilized its own expertise to identify the most relevant datasets. Based on preliminary discussion and analysis, the team included the following datasets in the study.

Primary Datasets

- 1) **Traffic Counts**. Highway 9 at Tiger Road is the only continuous traffic counter in Breckenridge. However, a temporary counter was placed at Main Street and Boreas Pass. This new counter allowed some additional monitoring, but longer-term tracking is necessary. 
- 2) **Trail User Counts**. Accurate, nearly complete counts are available for four primary trails in the Breckenridge area, providing 10 years of summer use data. The trail counter data show that outdoor recreation has been a major contributor to tourism growth in 2019-2022. 
- 3) **Sales Tax Collections**. Municipal sales tax collections are a key metric to understanding activity in Breckenridge. While they do not always correlate directly with visitation, they are a closely tracked and reliable data source that helps describe changes and growth in the local economy. 
- 4) **School Enrollment Numbers**. The Summit County School enrollment dataset was utilized to understand to what extent the population of school-aged children fluctuated over time. 
- 5) **Lodging Room Nights**. Lodging room nights help understand how long non-resident users are spending in the area. This metric is most helpful for helping to segment out to what extent changes in other metrics are attributable to day visitors versus overnight visitors. 
- 6) **Mobile location data (Cell phone data)**. Mobile location data collected passively through cell phone devices help tell a complete story of origins within town (e.g., local vs. visitors) and travel patterns through town over time. 

These data sources, all of which except mobile location data are collected by the Town, CDOT, and Summit County, are recommended as the key metrics at this time to inform the conclusions of this study. Other data sources are important to consider long-term (especially parking), but these data sources rose to the top of the list during the present analysis due to the following characteristics:

- 1) Current ease of access,
- 2) Consistency in long-term collection methods,
- 3) Relevancy to explaining current capacity of visitors and locals,
- 4) Long-term data reliability, and
- 5) Relevance to the questions most critical to the Town.

Description of Mobile Location Data

While not collected by the town or readily publicly available, mobile location data provides unparalleled insight into visitor movement and patterns and is the sixth key metric informing the conclusions of this



MOVEMENT_{by RRC}
Mobile Data Consulting

study. The research team employed RRC's *Movement* mobility data analysis process to vet and understand the trends and patterns that are not accessible via any other metrics.

For this project, the research team used the entire Town of Breckenridge boundary along with a buffer to include the ski resort in the metrics included. With this boundary, devices will be sampled if they enter the boundary of Breckenridge, no matter where they stay. Therefore, locals, commuters, and visitors alike are captured based on the device activity in the town and immediate surrounding area. A variety of cleaning procedures are used to further validate the data and eliminate any possible issues of inaccurate data.

Mobile location data refers to passively collected information from cellular devices. This data comes in the form of "pings" which are essentially individual cases or "breadcrumbs" that involve three key data points:

- 1) A randomly assigned and anonymous device ID.
- 2) The precise location of that device (latitude and longitude).
- 3) The exact date and time the device was observed at that location.

A variety of information can be gleaned from these pings, akin to GPS data but with much more depth and in greater sample sizes. For instance, the following metrics can be generated:

- 1) Device movement patterns in a study area.
- 2) Dwell time in an area (length of time spent in specified location).
- 3) Geographic origin (i.e., home) of users.
 - a. Inferred from the device's common location in the evenings for 3+ months at a time.
 - b. Allows for metrics to be split among locals/non-locals; in-state/out-of-state, etc.
 - c. Allows overlaying origin information with Census and other geographic data.

- 4) Temporal patterns by time of day, day of week, month (e.g., highest use periods and trends over time).
- 5) Spatial interaction with other onsite geographic data.

Other Datasets Investigated

The research team investigated the potential usefulness of other data sources for answering the key questions around capacity and use in the Breckenridge area. Other data sources considered, but not ultimately utilized for purposes of this current analysis only. These include:

- 1) **Housing Units.** The team assessed whether building permits or total units would be a strong predictor of capacity in the area. However, housing data is challenging to incorporate due to the various definitions of unit-types, differences in measurements between entities, and the lack of correlation with expected growth. The team believes that local residents are better captured through the mobility data at this time. Over time, there is definite potential in monitoring housing data, but it was deemed less practical for this study as the current trends with available data do not correlate with other resident-related data such as population, school enrollment, or the mobility data. Housing is a major topic of concern in mountain communities, but the varying definitions and classifications make it difficult to extrapolate to some other analyses. For now, we recommend avoiding using housing data to predict local population and instead use other proxies. Housing should be considered as a metric to track in terms of building capacity for employees and residents, but it currently does not correlate with population or volume.
- 2) **U.S. Census.** Census data is used across the U.S. to estimate demographics, population statistics, and various special topics. However, it is irregularly updated, making year-to-year analysis difficult.

While the American Community Survey provides some estimates at a yearly level, the data has been historically challenging to use in mountain communities. The nature of these communities (many commuting workers, high volume of second homes) makes some of the statistics difficult to interpret.

- 3) **Short-term Rentals.** Short-term rentals are a very controversial topic in many resort communities. Breckenridge has kept quite accurate records of short-term rental units, and there is no question that the volume of short-term rentals has been on the rise in recent years.

However, there has also been a concerted effort to ensure permit compliance. A new capacity limit on permits also influenced the number of homeowners who sought a permit, influencing the data and disallowing measurement of true growth vs. rise in permit compliance.

4) **Parking Counts and Free Ride data.** Correlating strongly with traffic counts, parking counts can be an important metric, as the act of parking can be a trigger for many residents to feel aspects of crowding. Not only do parking counts play into infrastructure challenges but also psychologically with residents' day-to-day lives. More real-time and thorough parking information could be particularly powerful if overlaid with other data sources, such as mobile device data. However, current parking and Free Ride data provides only intermittent and spotty coverage and is not incredibly useful in its current form. The challenges related to this data are outlined below:

a. **Parking data:** Currently, parking data is collected sporadically with AM and PM counts at various lots across the Town. While this data is useful in terms of monitoring lot occupancy across time, there are challenges with comparing over time. The current primary challenge is a lack of data to correlate lot usage over a given time. Most lots on peak days will be 100% full on both an AM and PM count, but the counts don't occur on every day and vary from year-to-year. For instance, July of one year may include two Saturdays while the next year may only include one. This makes it difficult to assess whether accurate comparisons over time are being made. A parking accumulation/turnover study is likely necessary to monitor patterns over peak days along with more real-time collection of lot volumes.

b. **FreeRide Data:** The FreeRide data is more apt to monthly comparisons since ridership is captured at a monthly level. The primary challenge with this data is the impact of COVID-19. Using the current data from 2017-2021, the data and routes are consistent from 2017-2019; however, 2020 and 2021 are severely impacted due to the COVID-19 pandemic. In 2020, many routes were cut, ridership significantly decreased, with the same pattern occurring in 2021. Because of the impact of COVID-19 on public transit numbers, the interpretation of the results can be misleading. At first glance, many would assume that visitation decreased significantly, but that is not the case. Instead, the hesitancy to use public transit and decrease in routes running confounds the data for the past two years.

Furthermore, Breckenridge has conducted various marketing efforts to try and encourage more public transit use. While this is a positive step in the right direction long-term, it further confounds the trend of FreeRide data and crowding/congestion. The same number of visitors may be present but more willing to use FreeRide services due to marketing, thus providing a false interpretation of the data. For these reasons, we cannot make a clear assessment of whether FreeRide data is correlated to crowding and congestion but monitoring use is still valuable.

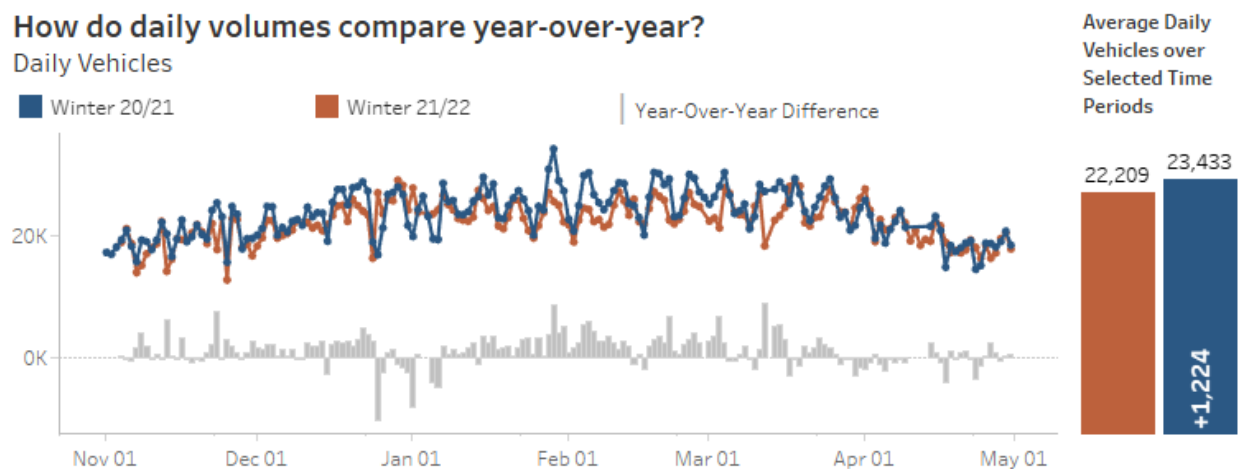
Analysis and Findings

Traffic Counts

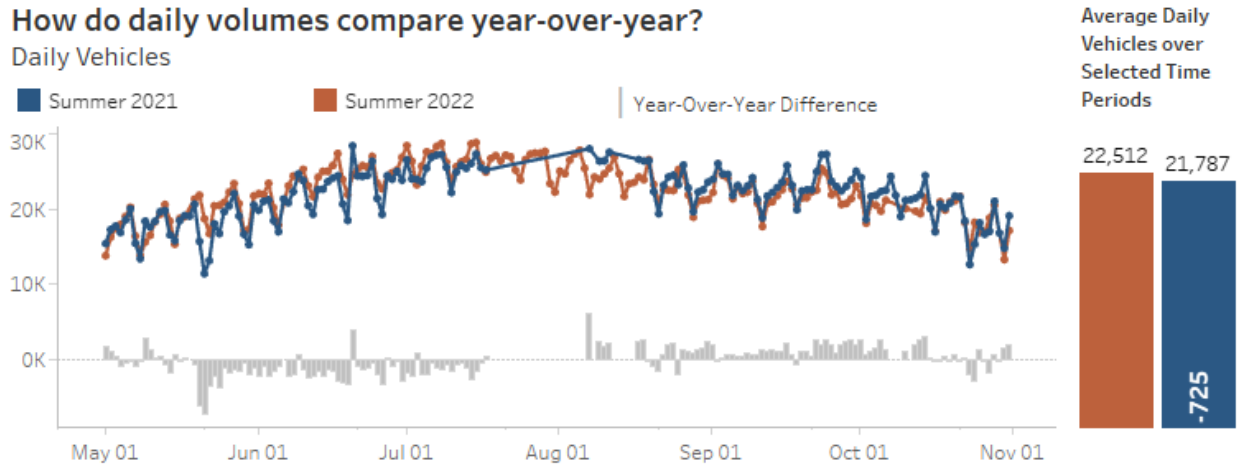
The traffic counter data for Tiger Road on Highway 9 was used to assess how the total volume of vehicles passing through Breckenridge has changed over the past 5+ years. The primary objective for this data was to assess how traffic volume has changed from pre- through post- COVID-19. As expected, 2020 was an outlier for evaluating various data types. Traffic was one of the most impacted metrics during the pandemic. This is likely due to a variety of factors including residents seeking out new activities outside, regional visitors seeking activities away from large groups of people, and a restriction of travel for a period of time for out of state visitors.

Displayed below are traffic counts from Winter 2020/21 vs. Winter 2021/22. According to the traffic counter collection on Highway 9, Winter 2021/22 was one of the busiest recorded, with Winter 2020/21 not far behind in traffic totals. Overall, both seasons were shown to be higher in average daily vehicles over the season compared to any previous year back to 2017.

Figure 1: Highway 9 and Tiger Road Traffic Data - Winter 2020/21 vs. Winter 2021/22

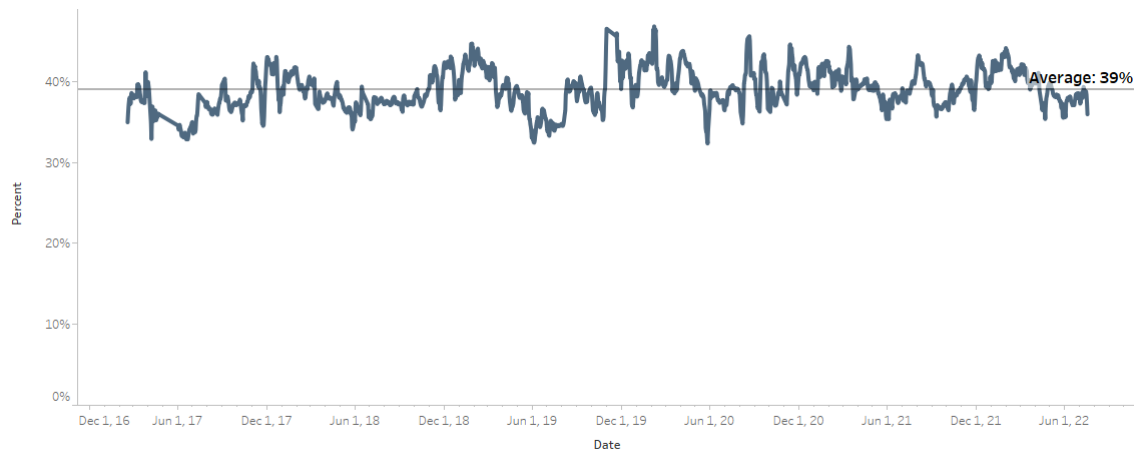


Summer 2022 traffic tells a different story. Compared to 2021, summer 2022 is down overall in terms of vehicle volume. However, average daily traffic is still higher than most previous seasons during the same time period.

Figure 2: Highway 9 and Tiger Road Traffic Data - Summer 2021 vs. Summer 2022


To further understand show how Highway 9 serves as a primary travel corridor for locals and visitors, the total counts of daily traffic were collected from CDOT at the I-70 Eisenhower Tunnel and Highway 9 at Tiger Road near Breckenridge. While I-70 is a massive, major travel corridor, it's noteworthy to show that the counts at Highway 9 can be fairly significant as well. On average, the traffic counts on Highway 9 can be nearly 40% of what is seen on I-70. That doesn't necessarily mean that 40% of I-70 travelers are going to Breckenridge, but rather that Highway 9 sees a large volume of traffic even when compared to a large interstate highway.

Additionally, the exits near Frisco rate as some of the most popular of users who are on the I-70 corridor. From a recent study with the I-70 coalition, winter and spring represent the most use in I-70 and 60% of the traffic occurs during peak times (Westbound Friday-Sunday and Eastbound Saturday-Monday). Overall, the I-70 corridor, especially near Frisco, is an incredibly busy area of travel with many visitors turning off to visit various sites near Breckenridge.

Figure 3: Highway 9 Traffic Volumes as a Percent of I-70 Eisenhower Tunnel Traffic Volumes: 2017-2022


Sources: CDOT Traffic Counts on Highway 9 at Tiger Road

Overall, traffic data has shown that volume has been significantly up since 2017. Seasonally, variation exists with Winter 2021/22 being one of the busiest on the road. This increase in traffic volume does place some strain on existing infrastructure, no matter the demographic of people using the road. Use does appear to be tapering down slightly as of Summer 2022, but a longer-term monitoring strategy is important. Moving into the future, a counter is being placed also on the southern end of Breckenridge on Highway 9. Historically, the traffic on this part of the highway has been unknown. With the growth in the Colorado Springs, CO area, understanding traffic patterns on this part of the highway is critical for the future.

Trail User Counts

Trail counter data is displayed below for the summer season. The four primary trails evaluated were: 1) B&B trail, 2) Toad Alley, 3) Sallie Barber, and 4) Shock Hill. These four counters had the most complete data and provide the highest level of insight into long-term trends. Winter data was only available and complete for the B&B and Sallie Barber.

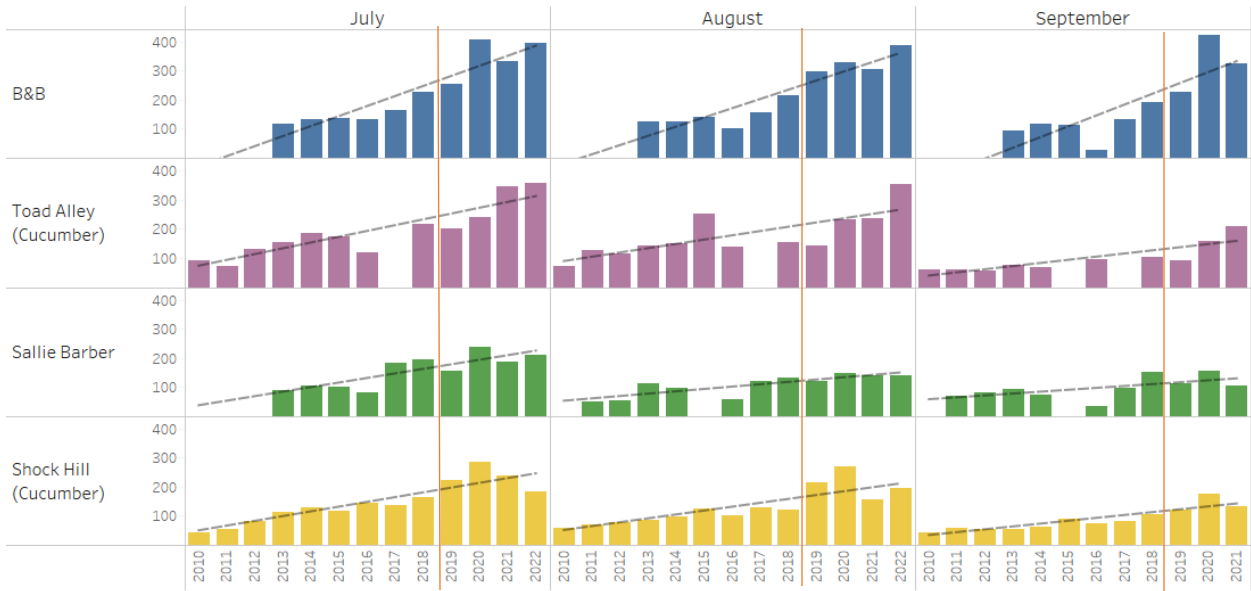
Trail count data over time shows a dramatic increase in the average daily counts since 2010. Since approximately 2016/17, average daily counts have increased year-over-year and at times at a significant level. The B&B and Toad Alley trails have seen some of the more dramatic increases from the peak summer season. For instance, the B&B trail was seeing average counts around ~150 people in July 2013. By 2020, almost 400 people were observed per day, an increase of 166% (Note the B&B trailhead parking lot was expanded in 2019 to accommodate additional users).

During winter, the B&B and Sallie Barber trails both have trail counts active for the past number of years. Similar to summer, both trails have seen increases over time with the B&B trail seeing the steepest jump in counts. Again, this continues to indicate overall growth and is likely a spot where the increases are most obvious since winter trail use used to be quite sparse.

The parking lot expansion at the B&B trailhead in 2019 likely led to more visitors being able to access the site. Increasing parking capacity likely contributed to an increase to trail counts. While expanding parking may be a benefit to most users, trail encounters (e.g., number of other users that people see on the trail) is a major factor to the perception of crowding. Outdoor recreation studies have been tracking encounters for years as it has shown to have an impact on the overall experience. Increases in trail counts have been seen across the board at trails in the Breckenridge area.

Figure 4: Average Daily Trail Counts (Summer season)
Average Daily Trail Counts

Summer (July - September), Selected Locations

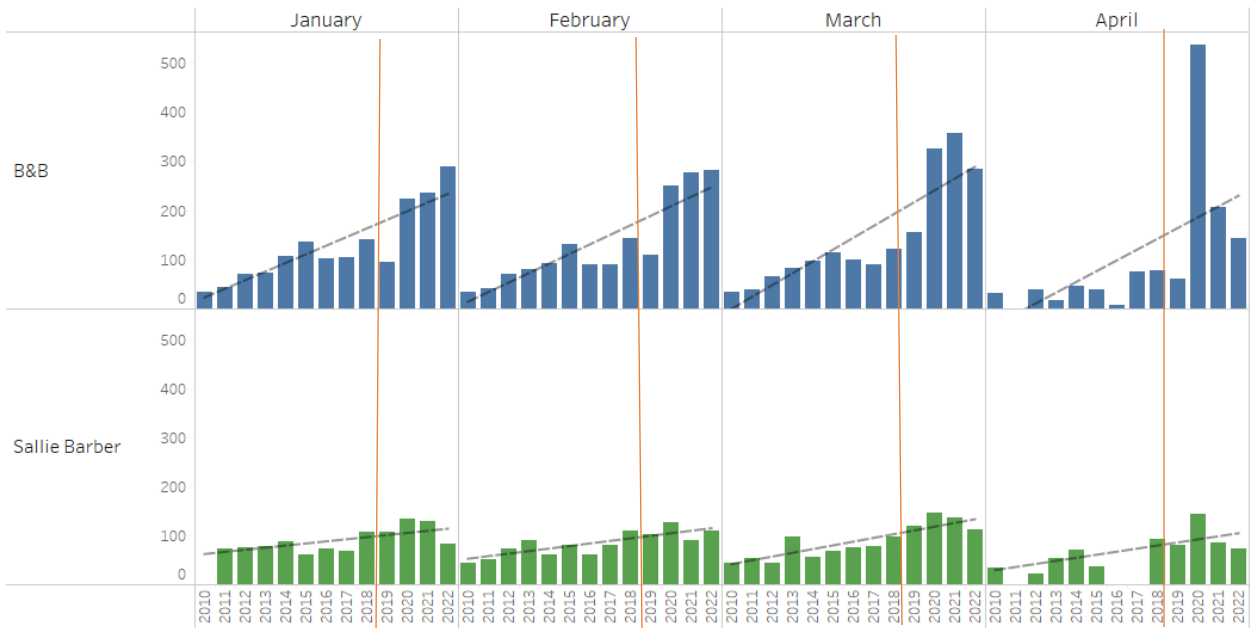


Source: Town of Breckenridge Trail Count Summary

*Note: Orange vertical line indicates when parking lot expanded on B&B trailhead in 2019

Figure 5: Winter Trail Counts, B&B and Sallie Barber, 2010-2022
Average Daily Trail Counts

Winter (January - April), Selected Locations



Source: Town of Breckenridge Trail Count Summary

*Note: Orange vertical line indicates when parking lot expanded on B&B trailhead in 2019

Sales Tax Collections

Sales tax collections at the community level is displayed below. Contrary to initial thoughts, the COVID-19 pandemic was not as challenging for municipal sales tax figures in some locations as much as others. Breckenridge has seen a substantial increase in sales tax collections in both 2021 and current 2022. In fact, December (2021), January, February, and March of 2022 saw the highest grossing sales tax collections since 2014. An obvious dip occurred in collections during March, April, May of 2020, but the rebound since then has outpaced any other year on record (minus some outliers). This overall boom in collections has fueled discussions about its cause. Using the other available data in our analysis, it seems relatively clear that an increase in visitation after the height of the pandemic has directly led to such an increase.

Figure 6: Breckenridge Sales Tax Equivalent (2014 – September 2022)

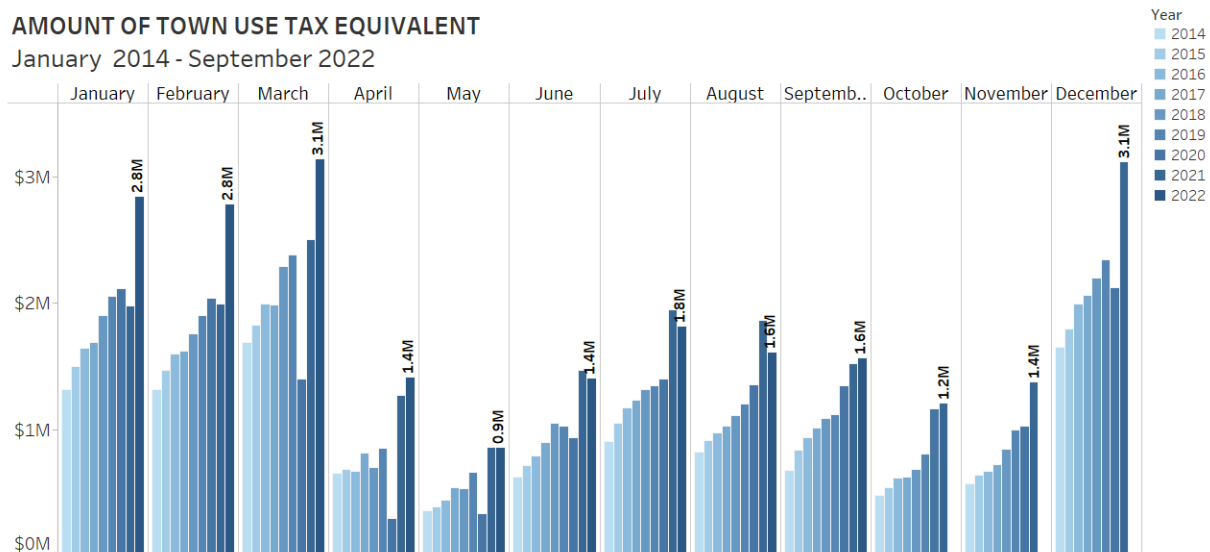
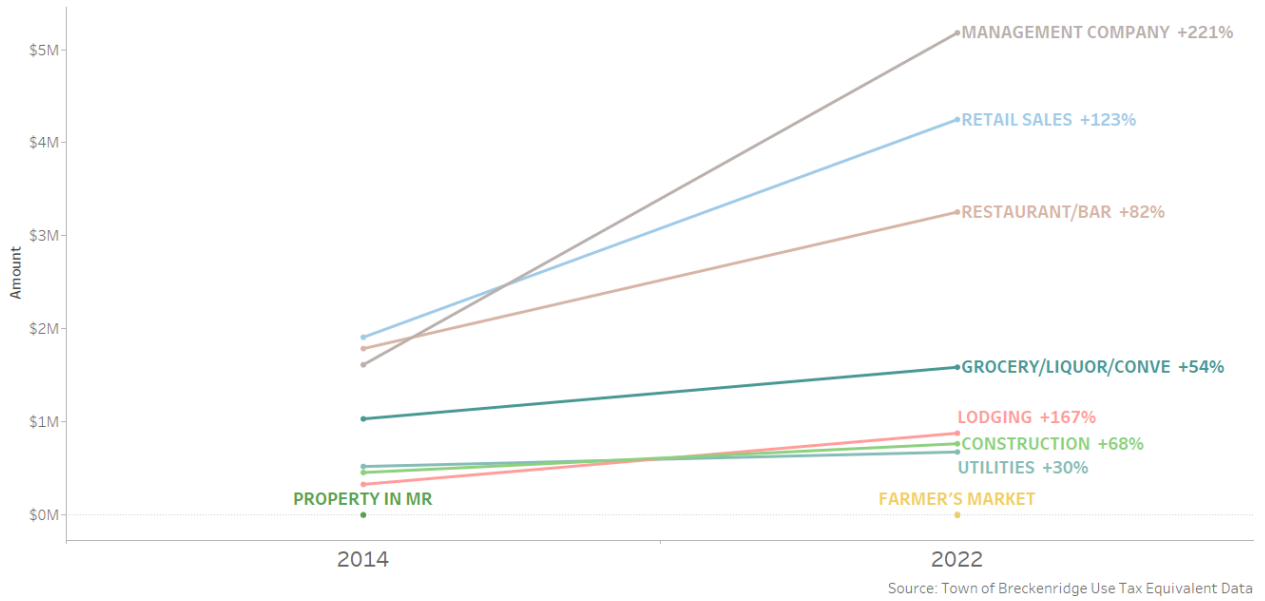


Figure 6 displays the composition of sales tax equivalent in Breckenridge among various business sectors. Measuring from 2014 to 2022, the largest sector growth is property management companies (which includes mostly multi-unit organizations) which at +221% jumping to well over \$4 million in year-to-date funding. Lodging (hotels, bed and breakfasts, lodges) comes in second at +167% with retail sales (+123%) and restaurant/bars (+82%) coming in just behind. Overall, all sectors have significantly increased, well past the point of inflationary effects.

By season, winter sales tax figures tend to be significantly higher than summer season (or shoulder seasons), especially in the past two years. As a seasonal metric, it would be expected that this trend would continue unless the summer season becomes a much larger spending period for visitors. Winter season draws visitors for a longer period of time with most skiing and spending money on various purchases throughout their time in the area. Summer season, while still busy, draws a lower sales tax equivalent. Visitors may be more likely to hike or participate in free activities than during winter, contributing to less overall spending.

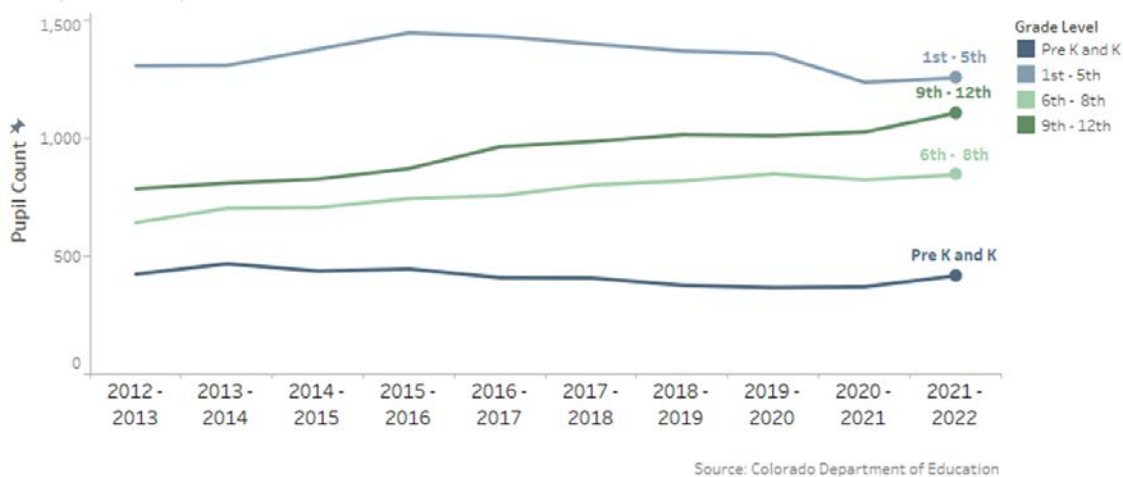
Figure 7: Amount of Town Use Tax Equivalent - Selected Business Activities
AMOUNT OF TOWN USE TAX EQUIVALENT - SELECTED BUSINESS ACTIVITIES

September 2022 VS 2014



School Enrollment Data

According to current data from Colorado Department of Education, school enrollment has remained rather consistent over the past 10 years. Some fluctuations exist such as a slight dip in Pre-K and Kindergarten. Furthermore, 1st-5th has seen some decreases since 2020. However, 9th-12th has increased slowly at the same time. Therefore, all signs point to stability in terms of a typical resident population in the area. Couple this with Breckenridge's official population totals, the resident base has held quite steady despite a significant shift in cost of living, home prices, and mountain towns overall.

Figure 8: Summit County School Enrollment - 2012/13 - 2021/22


Lodging Room Nights and ADR

Lodging within the Breckenridge area has remained consistent, with a decrease in 2020 due to COVID. By season, average daily room nights have varied throughout the years. For Winter, from 2015/16 the average daily room nights have increased with the exception of Winter 2019/20 and 2020/21, the rebound from COVID. However, Winter 2021/22 was the strongest season on record. For summer, the average daily room nights have remained much more static with Summer 2016 starting at 1,328 and only increasing to a high of 1,448 in Summer 2018. As of 2021, the average daily room nights have remained consistent at 1,447, suggesting a rebound from COVID, but reliable results.

Overall, room nights have grown over time with the past season (Winter 2021/22) being the largest on the books despite the downturn from COVID in 2020 and early 2021. This metric appears to be important as it directly tracks use of rooms over time and therefore, visitors in Town. Monitoring average room nights over time should be considered a key metric to measure moving forward.

Figure 9: Breckenridge Average Daily Room Nights, 2015 – Winter 201/22

Average Daily Room Nights

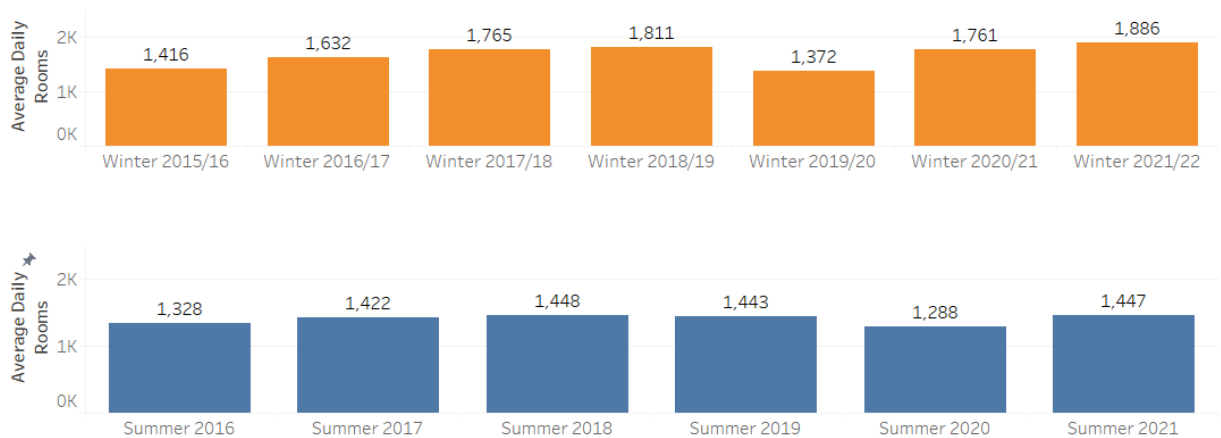
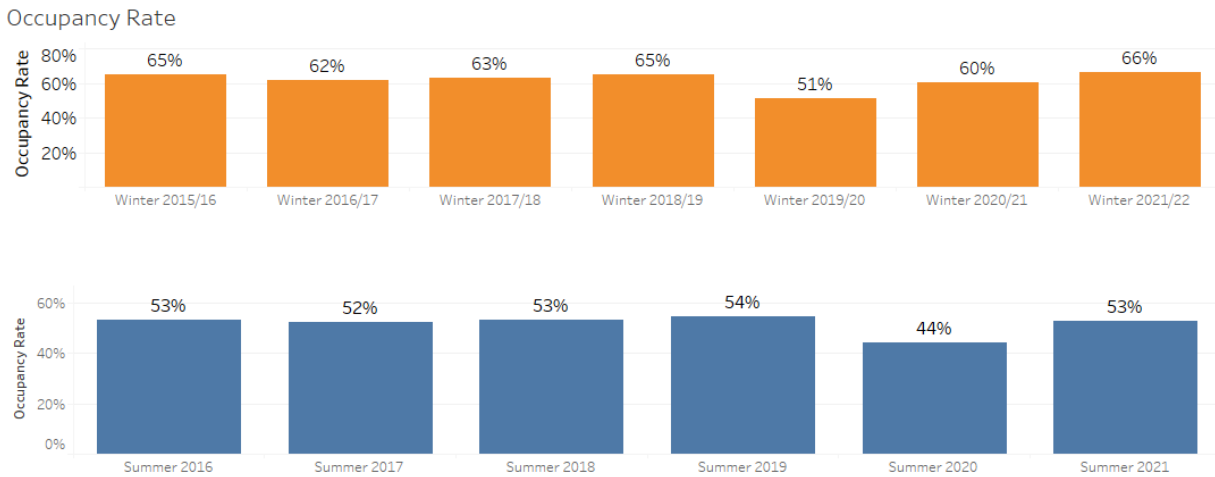
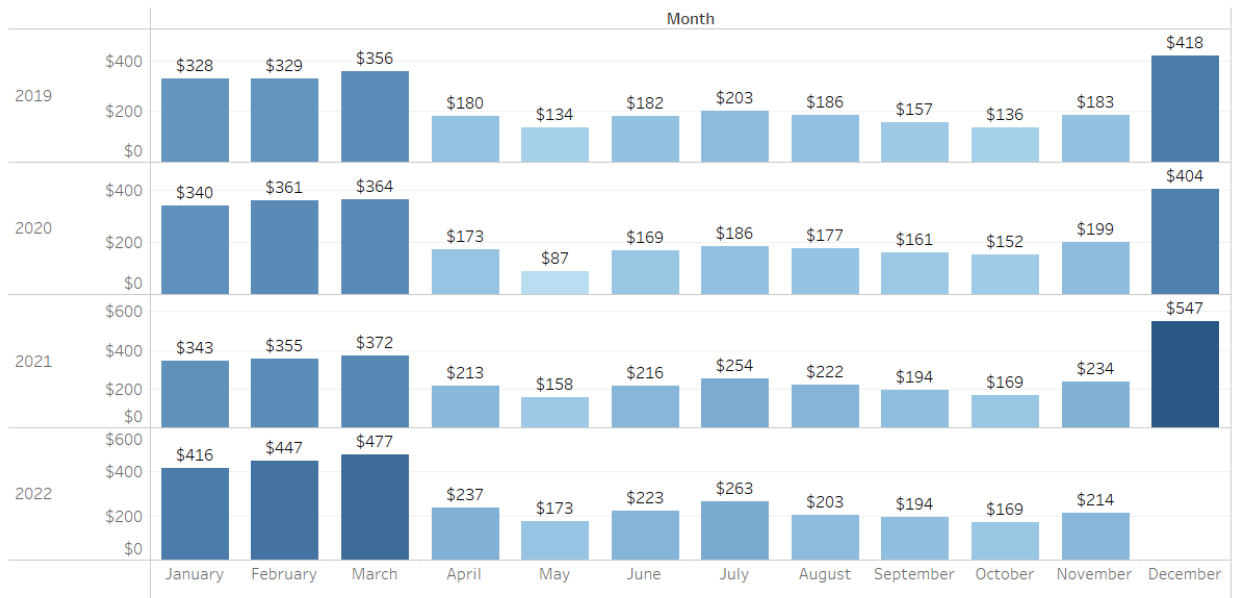


Figure 10: Breckenridge Occupancy Rate Per Season, 2016 – Winter 2021/22


Average Daily Room Rate (ADR) is displayed in the graph below. ADR has grown over time, while dipping during the peak of the COVID-19 pandemic but rising to a four year high in 2022. As of December of 2021, ADR peaked at \$547 a night with summer growing more modestly to \$263 a in July 2022. Shoulder seasons (April-May and September-November) have not seen as sharp of increases, but ADR in every month in 2022 is significantly higher than 2019 and most of 2021, especially during the winter season.

Figure 11: Average Daily Breckenridge Room Rate, 2019 - November 2022

Town of Breckenridge - Average Daily Room Rate (2019 -2022)



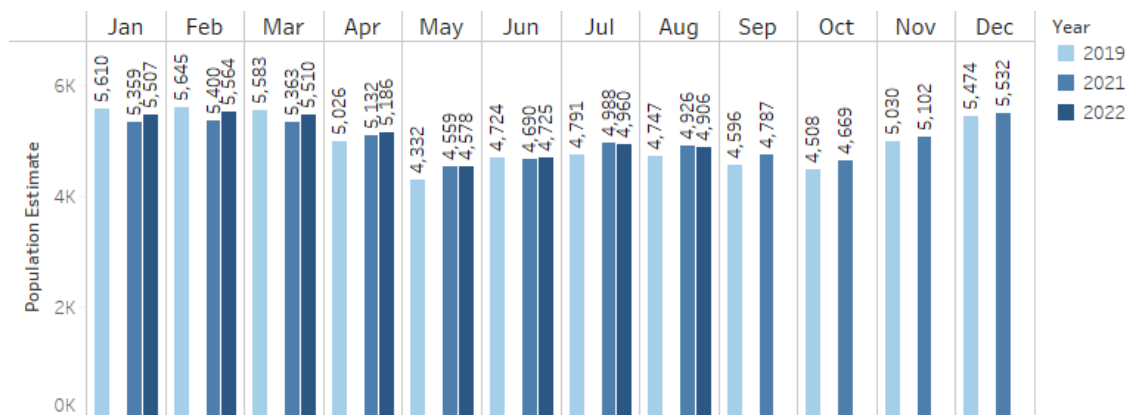
Mobile Location Data

While mobile location data (cell phone data) provides a wealth of insight into the origins of residents and visitors to Breckenridge, it should not be solely relied upon to estimate total visitation or evaluate year-over-year trends in volumes of visitation. Although the use of smart phones is nearly ubiquitous among the travelling adult public, mobile location data providers have access to different volumes of data at different times. As such, mobile location data represents a sample (albeit a large and diverse sample) and not an accurate indicator of total use.

To estimate total visitation to the Town of Breckenridge, RRC utilized mobile location data to understand the composition of devices observed within Town boundaries. All devices within the Town of Breckenridge are counted and examined in this dataset. The figure below highlights the area used as a geographic boundary for Breckenridge (note: a buffer is required due to GPS error and erroneous data ping cleaning). Of the total devices observed in Breckenridge, we determined what share were local residents, what share commuted into Breckenridge for work, and what share were visitors. We then used American Community Survey (ACS) and Quarterly Census of Employment and Wages (QCEW) data to understand the size of the local population. Together, these three data sets enable us to estimate the total annual visitation to the Town of Breckenridge.

The ACS estimates the Breckenridge resident population for April of each year; however, popular tourist destinations such as Breckenridge experience seasonal fluctuations in population. In order to account for these seasonal shifts, RRC utilized monthly job count data for Summit County from QCEW. Combined, these two datasets provided an estimate of monthly population for the Town of Breckenridge for 2019, 2021, and 2022.

Figure 12: Estimated monthly population based on ACS and QCEW data, January 2019 – July 2022

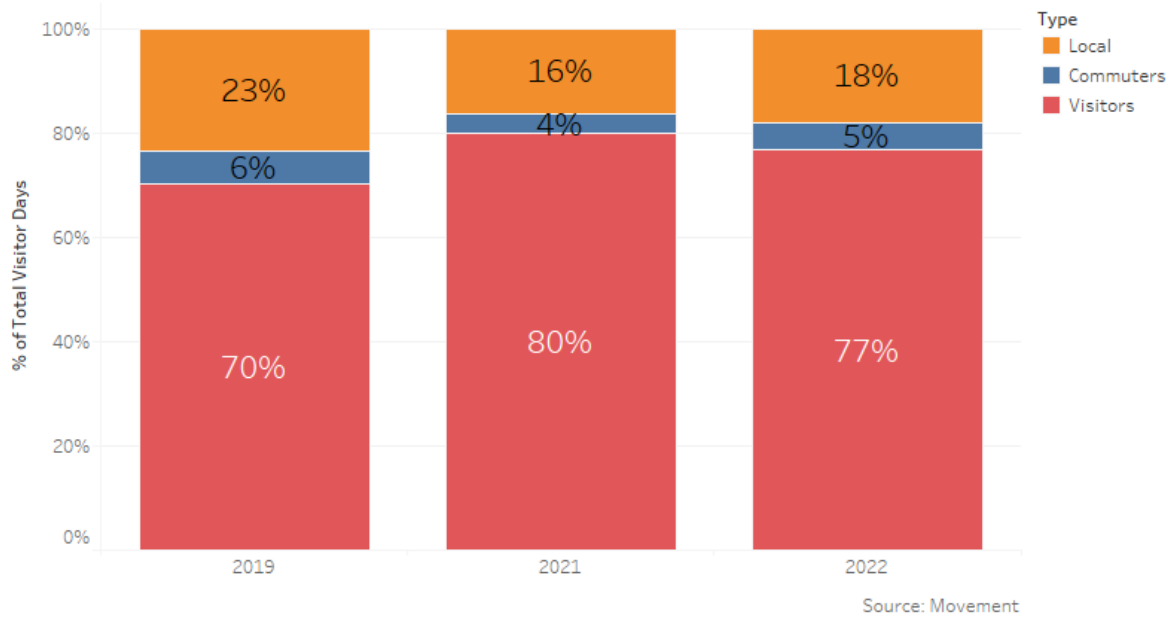
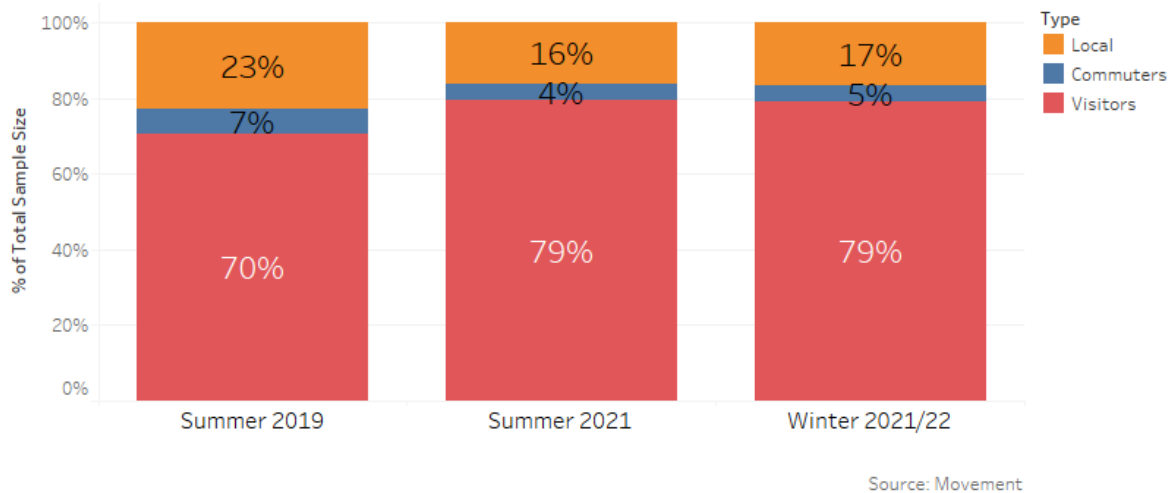


Source: American Community Survey and QCEW

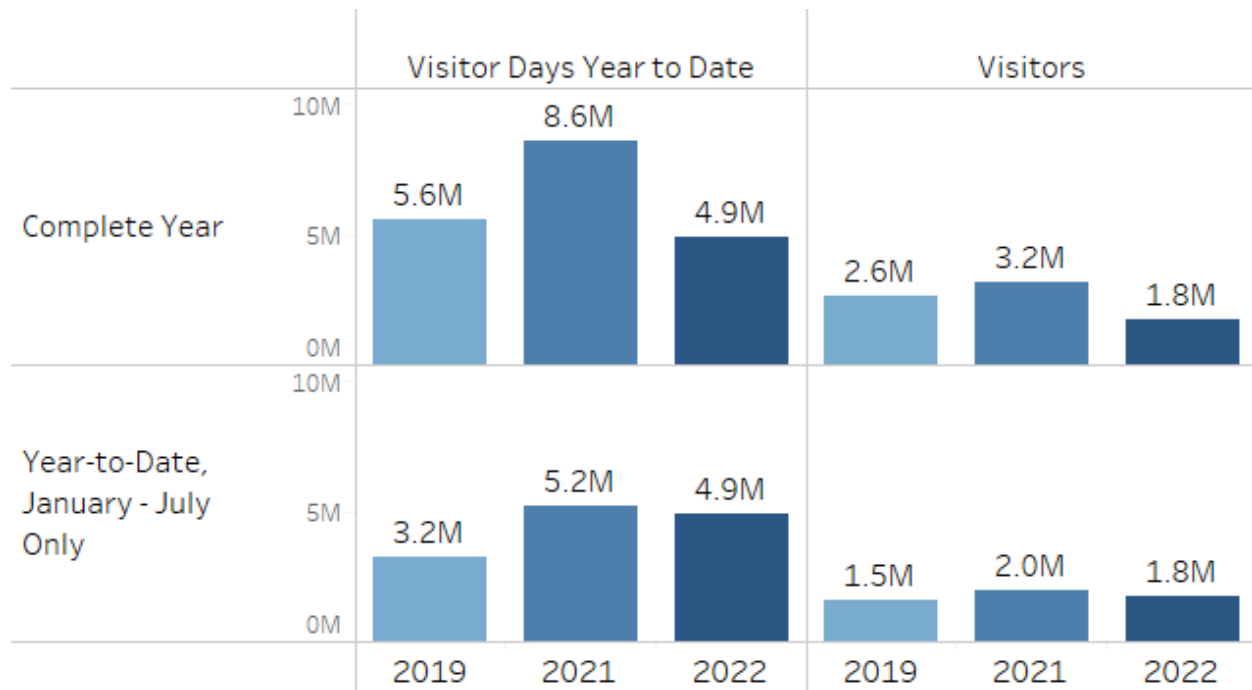
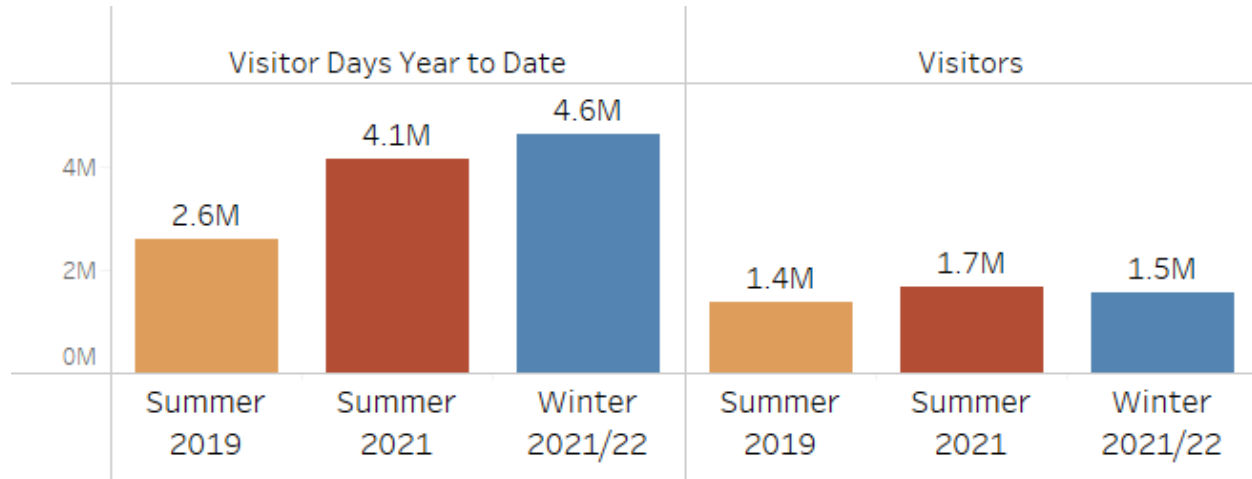
As evidenced in the figure below, Breckenridge experienced a notable increase in the percent of total devices estimated to be visitors across the past 3 years. In 2019, 7 in 10 people in Breckenridge were visitors. In 2021, that number increased to 8 in 10.

Figure 13: Share of mobile devices by visitor type, January 2019 – July 2022

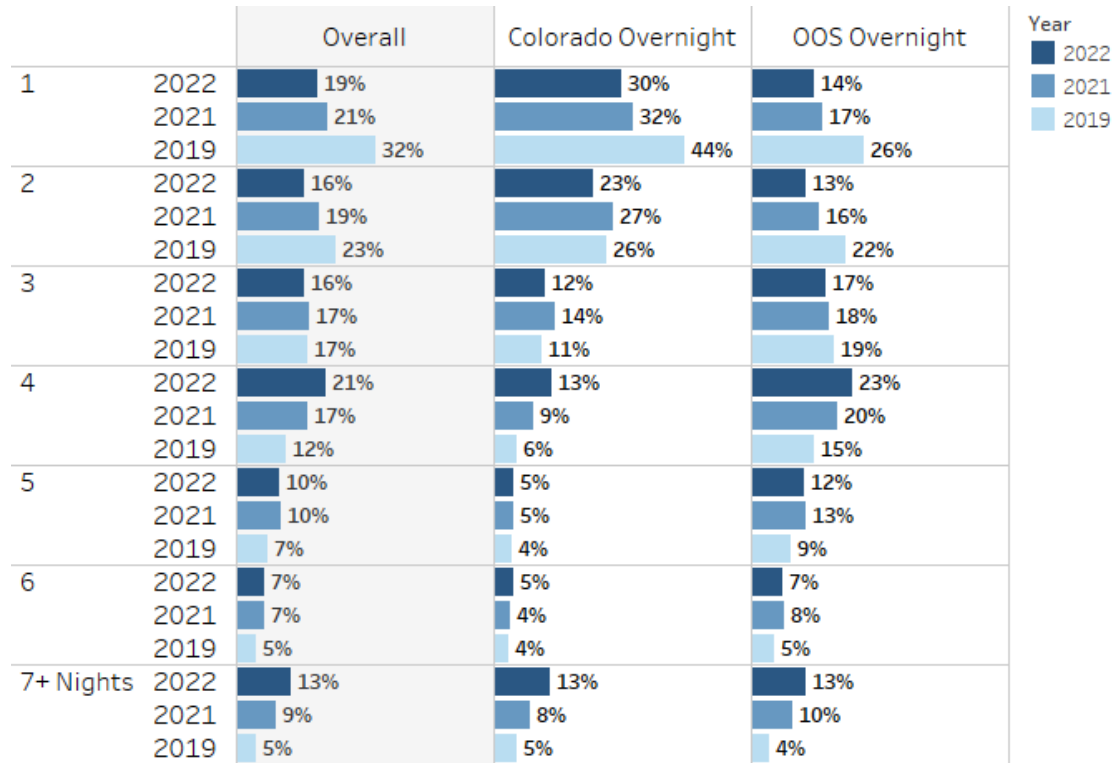
Composition of Devices Observed Within the Town of Breckenridge


Figure 14: Seasonal Shares of Visitor Segments (For complete seasons only, 2019-2022)


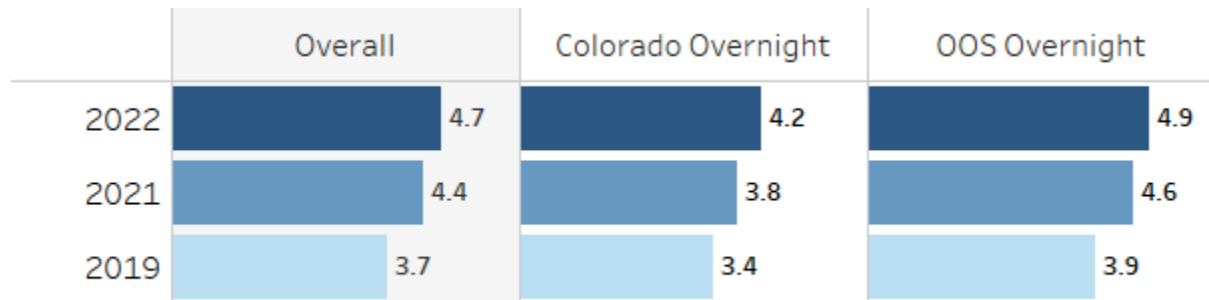
The known percentage of locals from our mobile location data was then used with the known volume of locals to estimate the volume of total visitors to Breckenridge. Overall, the number of visitors has remained relatively consistent with some increases in 2021. For seasons with complete data, the trends match up on a seasonal basis as with the overall results as well.

Figure 15: Estimated visitors and visitor days by year, January 2019 – July 2022

Figure 16: Seasonal Visitation Metrics (For complete seasons only), 2019 - 2022


In addition to increasing visitor day estimates, mobile location data enables us to understand the length of stay. Figures 17 and 18 display various length of stay statistics, which have changed over time. Since 2019, the length of stay among both Colorado and out of state overnight has increased. This is likely due to capturing more second homeowners and those staying in longer-term accommodations such as timeshares and short-term rentals. Typical survey data does not pick this kind of information up as easily, but there has been somewhat of an increase in length of stay over time in Breckenridge. The COVID-19 pandemic transformed the ability to travel for longer stays, which is likely the cause of the change presented.

Figure 17: Estimated overnight visitor trip length in nights, January 2019 – July 2022


Source: Movement

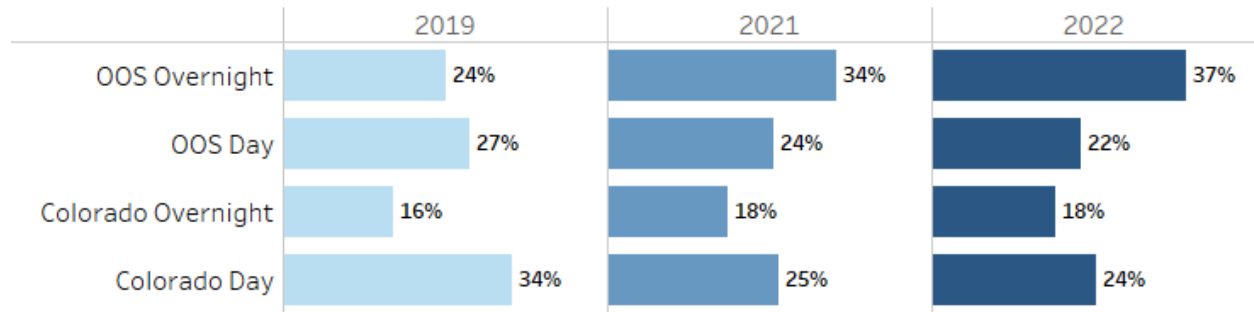
Figure 18: Estimated average overnight visitor trip length (nights), January 2019 – July 2022


Source: Movement

*Note: OOS = “Out of State” visitor living away from Colorado.

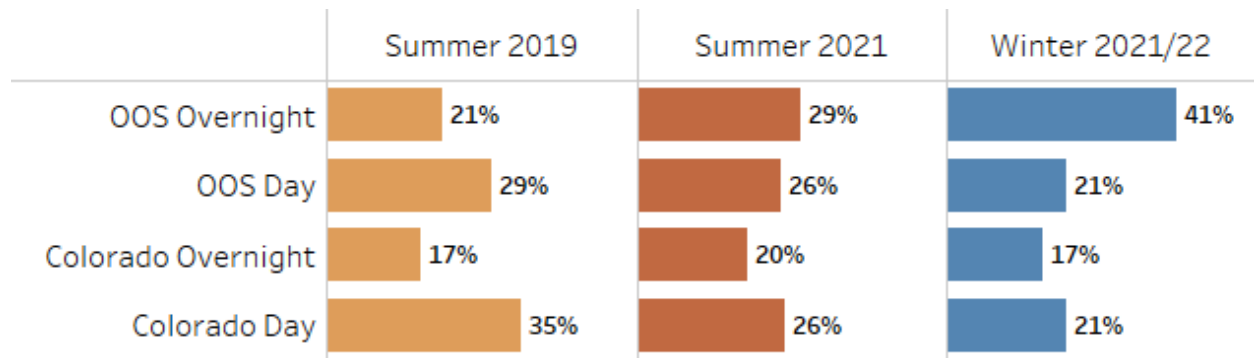
Figure 19: Share of visitors to Breckenridge by Visitor Type, January 2019 – July 2022

Visitor Type



Source: Movement

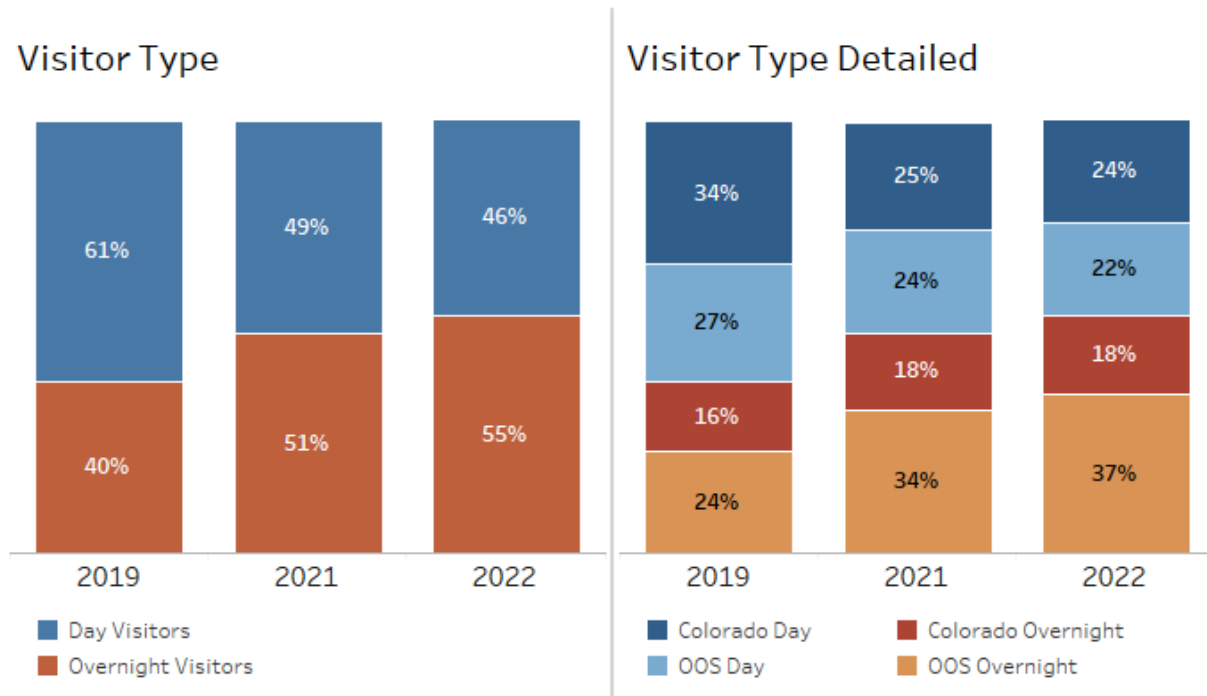
*Note: OOS = “Out of State” visitor living away from Colorado.

Figure 20: Seasonal Share of Visitor Types (For complete seasons only), 2019 - 2022


Source: Movement

*Note: OOS = “Out of State” visitor living away from Colorado.

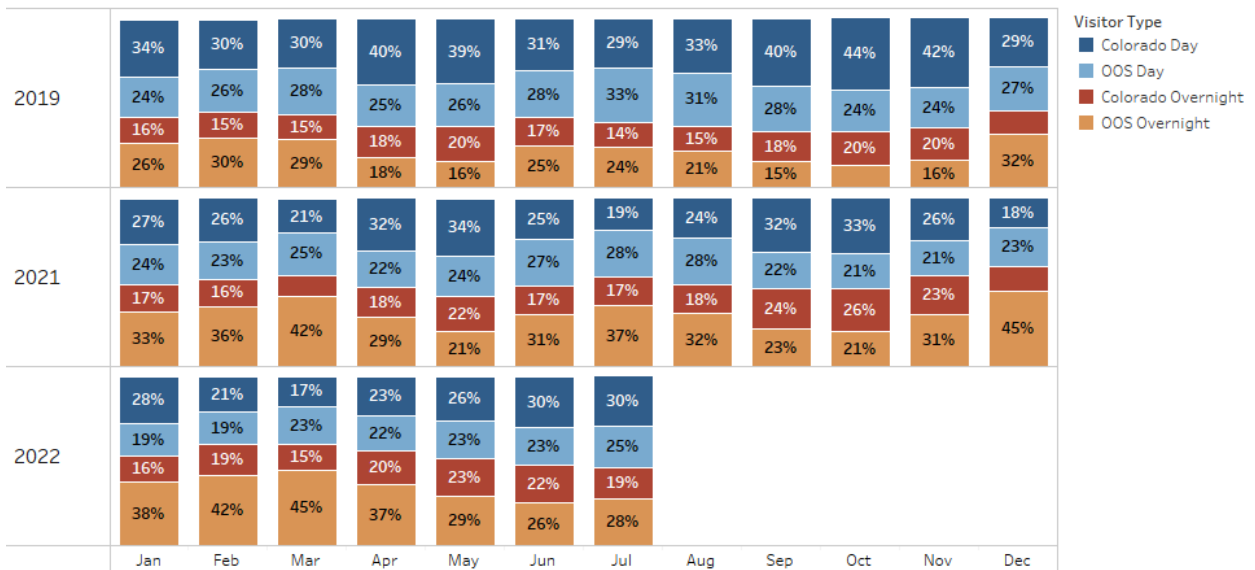
Figure 21 below highlights the different shares of visitor types. The share of overnight visitors in 2019 was approximately 40% and has grown to 55% in 2022. During the busiest year (2021), the split was nearly equal between day (49%) and overnight visitors (51%). However, when further examined, the share of out of state overnight visitors increased between 2019 to 2022, jumping by 13 percentage points to 37%. Further highlights are provided by month in Figure 22.

Figure 21: Breckenridge Visitor Type Breakdown


*Note: OOS = “Out of State” visitor living away from Colorado.

Figure 22: Visitor Type by Month of Arrival (2019-2022)

Visitor Type by Month of Arrival



Source: Movement

*Note: OOS = “Out of State” visitor living away from Colorado.

Study Limitations:

All research studies have limitations based on various factors. This project shares in those limitations. To be as transparent as possible, we acknowledge the following limitations:

1. The interpretation and conclusions from this study are based on the most recent and available data. We acknowledge that not all datasets are feasible to be considered for this study.
2. The COVID-19 pandemic introduced a variety of anomalies into various data sets. There is the possibility that trends seen in the past five years may change over time due to a variety of factors.
3. Some demographics of the Breckenridge area may be underrepresented due to various data collection methods (e.g., temporary workforce).
4. Despite data showing growth or decline in specific groups, it is unknown to what extent these changes impact the psychological factors of Breckenridge residents.

These are but a few limitations of this study that are acknowledged and present in any social science research study.

Conclusions:

Using all the relevant data sources along with a thorough analysis of overall volume using mobile location data, the primary conclusions reached by the research team are below:

- 1) Overall use and volume of people in Breckenridge have significantly increased since the mid-2010's. Our team used 2017 as a baseline for most analyses, but some datasets show significant growth from 2010 – 2017 with even more increases from 2017 on. From 2017 on, data is much more readily available to understand overall changes. Traffic, trail use, and lodging room nights all point to a sustained, increased growth through 2021. However, 2022 looks to be moderating the demand and growth in use to a degree. There's no question that 2021 was the busiest year likely in Breckenridge's recent history, but there might be somewhat of a course correction in 2022. Although overall it is still quite a busy year when compared to the mid 2010's. Long-term monitoring of these data sources will help flag relevant changes and modifications over time.
- 2) At this time, an increase in residents is very likely not the primary cause of growth or strongly contributing to the impacts from increased perceptions of demand. All available data indicates stability in the immediate local resident population; however, there has been some growth in nearby communities within Summit County (e.g., Silverthorne). Even then, population growth has remained relatively stable over time with some shifts back and forth. The behavior of some residents may have changed over time (e.g., more Free Ride use, additional

remote work options), but the volume of these changes are low and are not likely to impact overall perceptions.

- 3) According to the mobility data analysis, the share of out of state visitors has continued to increase since 2019. Moreover, the total number of estimated visitor days increased significantly between 2019 to 2021. An increase in length of stay along with a further uptick in 2021 in estimated unique visitors likely adds to the perception of crowding. As stated, 2022 figures seem to be down when compared to 2021. It's unclear at this time, but 2021 may be an outlier due to impacts from COVID-19. However, it will be critical to monitor how these figures change moving forward into 2023 and beyond. Monitoring how these datapoints change over time will be important to understand if the trend has reversed to some degree.

Overall, the perceptions of increased number of people in the Breckenridge area seem to be valid. The number of people in the area have increased by most available metrics, and this increase seems to be from those living outside of the community. However, Breckenridge still relies on tourism and as the slate of recent resident sentiment surveys suggests, the large majority of residents understand and appreciate that. The analysis and framework laid out in this document suggest using multiple data sources and careful analysis to monitor how things change into the future. With hints of economic downturns, it's important to use all available data moving forward to ensure Breckenridge both benefits from tourism and creates a high quality of life.